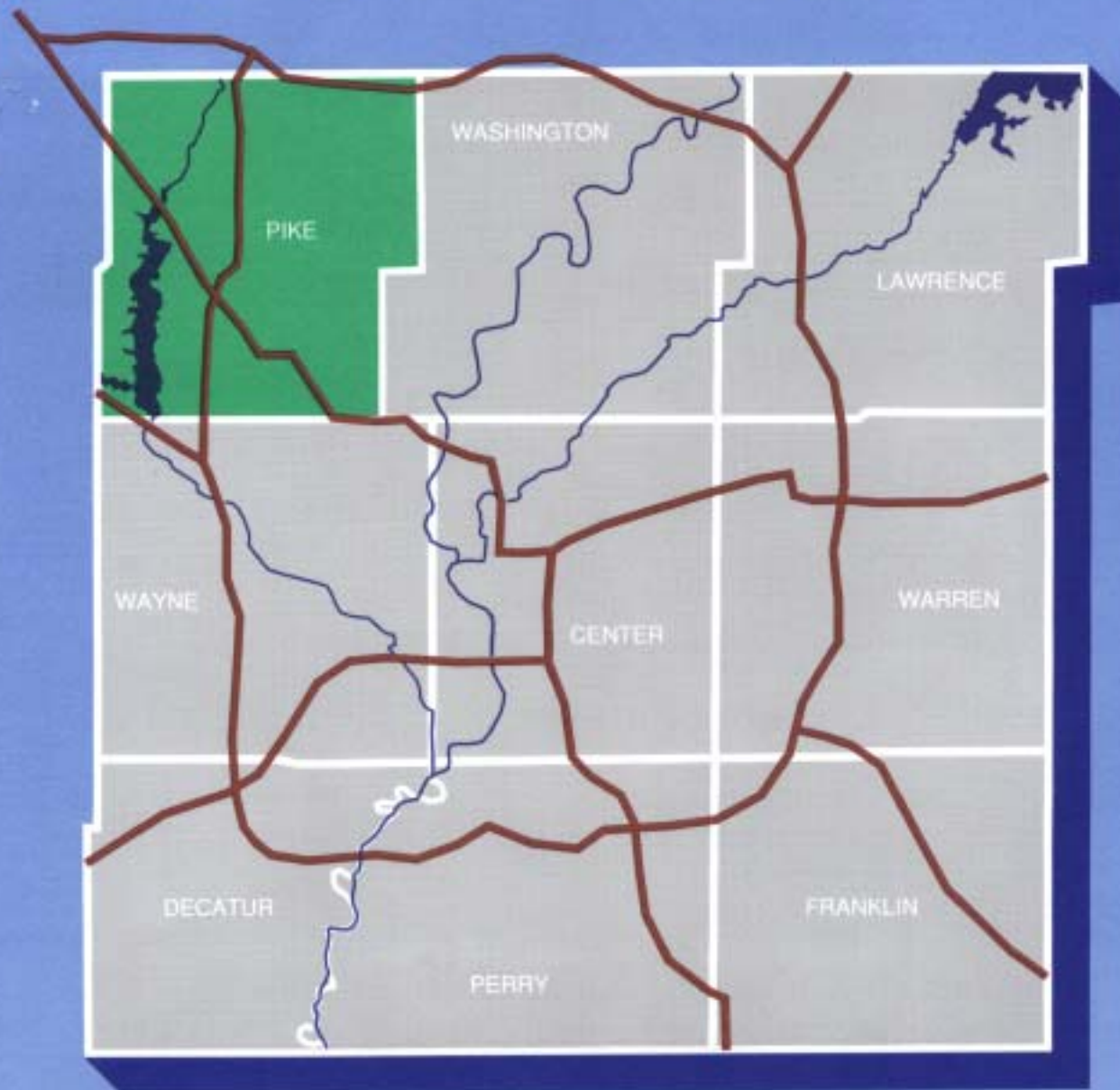


PIKE TOWNSHIP



COMPREHENSIVE LAND USE PLAN

93-CPS-R-2

ADOPTED FEBRUARY 17, 1993

DEPARTMENT OF METROPOLITAN DEVELOPMENT
PLANNING DIVISION
CITY OF INDIANAPOLIS-MARION COUNTY, INDIANA



STEPHEN GOLDSMITH

Pike Township

Comprehensive Land Use Plan



Prepared by:

Department of Metropolitan Development

Division of Planning

Indianapolis-Marion County, Indiana

Adopted February 17, 1993

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Pike Township Comprehensive Land Use Plan

Introduction

Pike Township, located in northwestern Marion County, has been one of the fastest growing townships in the county since 1960. Development is affected by Pike Township's notable natural and manmade features, including Eagle Creek Reservoir, Eagle Creek Park, and varied rolling terrain. The township is also characterized as having excellent highway access (it is traversed by three interstate highways), and a diverse economic base.

Detailed data on population, land use, zoning, transportation, schools, utilities, and other township characteristics are included in the *Pike Township Comprehensive Planning Study Data Book*. The *Data Book* was compiled by the Department of Metropolitan Development's Division of Planning in 1987.

To initiate the planning process for Pike Township, the Division of Planning widely distributed the *Data Book*. A Pike Township Planning Committee was formed to generate ideas and make land use recommendations for the Pike Township Comprehensive Plan revision. The Planning Committee meetings were conducted in a town meeting fashion, and were open to anyone who wished to attend. The meetings resulted in the land use recommendations for the Pike Township Comprehensive Land Use Plan Map, adopted in October, 1990.

This Comprehensive Plan narrative contains a summary of Pike Township's demographic trends, land use issues, and planning and development goals; a list of Thoroughfare Plan priority improvements for township roadways; and a section detailing the stages of urban development and critical areas in the township.

The Pike Township Comprehensive Plan revision, a part of the Comprehensive Plan for Marion County, is comprised of this narrative, its accompanying Pike Township Comprehensive Land Use Plan Map, and the Official Thoroughfare Plan for Marion County. The information, policies, and recommendations that make up this Comprehensive Plan document should be viewed in the context of these previously adopted documents, and the Marion County Comprehensive Plan narrative, which was revised and adopted in October, 1991.

Township Population and Land Use Characteristics

- *Pike Township is growing rapidly relative to the balance of Marion County.*
 - In 1960 fewer than 6700 people resided in Pike Township, accounting for slightly less than one percent of the entire Marion County population. But by 1990 Pike Township's population accounted for nearly six percent of the County. Prior to this period, Pike Township was largely undeveloped -- urban growth and development had virtually bypassed the northwest area of Marion County.
 - In 1990 (as was also the case in 1970 and 1980) Pike Township was the fastest growing township in Marion County. The rate of population growth, (78%), was more than double the growth rate of any other township in Marion County. Additionally, no other township experienced a greater absolute increase than Pike over the last decade. The population increased by nearly 20,000, from 25,336 in 1980 to 45,204 in 1990.
 - Since 1973, undeveloped land has been used rapidly. Roughly 40% of the township's land remained undeveloped or was farmland in 1991; that figure compares with more than 51% in 1985, and about 63% in 1973. If development continues at the rate of recent decades, then Pike Township would be fully developed in approximately 31 years, or by the year 2022.
 - According to the 1990 Census, Pike Township averaged 2.21 persons per household, which is less than the average for all of Marion County (2.45 persons). This number may be lower due to a relatively high proportion of multi-family housing units which generally have lower number of occupants per unit than single-family units. However, the predominant growth in the most recent years appears to be in single-family developments. Generally, more people per household means more school-age children per household and theoretically, an increased demand on schools. However, since 1970, Pike Township's school-aged population has been declining as a percentage of the total population.

Comprehensive Plan Response - The Comprehensive Plan recognizes that areas of the township are in various "stages of development." Therefore, the plan recommends policies that (1) prevent fast-developing suburban areas from overburdening school, roadway, sewer, and water systems, and (2) lower development costs in rural areas by encouraging them to develop after such systems are already in place.

Table 1

Pike Township **Comparisons of Existing Land Use and Comprehensive Plan Recommendations**

| Land Use Category | 1991 Existing Land Use | | 1984 Comprehensive Plan Land Use Recommendations | | 1991 Comprehensive Plan Land Use Recommendations | |
|-------------------------------|------------------------|-----------|---|-----------|---|-----------|
| | Acres | % of Twp. | Acres | % of Twp. | Acres | % of Twp. |
| Residential | | | | | | |
| Single & Two Family | 4664 | 16.7 | 11116 | 39.8 | 10647 | 38.1 |
| Medium Density | 1311 | 4.7 | 3233 | 11.6 | 2121 | 7.6 |
| Total | 5975 | 21.4 | 14349 | 51.4 | 12768 | 45.7 |
| Commercial | | | | | | |
| Office | 359 | 1.3 | * | * | 1250 | 4.5 |
| Retail | 1157 | 4.1 | * | * | 1295 | 4.6 |
| Total | 1516 | 5.4 | 1741 | 6.2 | 2545 | 9.1 |
| Industrial | | | | | | |
| Light | 1396 | 5.0 | 2401 | 8.6 | 2051 | 7.3 |
| Heavy | 621 | 2.2 | 1583 | 5.7 | 1642 | 5.9 |
| Total | 2017 | 7.2 | 3984 | 14.3 | 3693 | 13.2 |
| Public/Semi-Public | | | | | | |
| Parks** | 4752 | 17.0 | 4547 | 16.3 | 5108 | 18.3 |
| Roadways | 1651 | 5.9 | 1651 | 5.9 | 1651 | 5.9 |
| Eagle Creek Airport | 300 | 1.1 | 336 | 1.2 | 335 | 1.2 |
| Other*** | 526 | 1.9 | 1312 | 4.7 | 1312 | 6.5 |
| Total | 7229 | 25.9 | 7846 | 28.1 | 7846 | 31.9 |
| Vacant or Agricultural | 11183 | 40.1 | **** | **** | **** | **** |
| Total, All Categories | 27920 | 100.0 | 27920 | 100.0 | 27920 | 100.0 |

* The 1984 Comprehensive Plan did not differentiate between office and retail commercial uses.

** This category includes 1300 acres of surface water in Eagle Creek Reservoir.

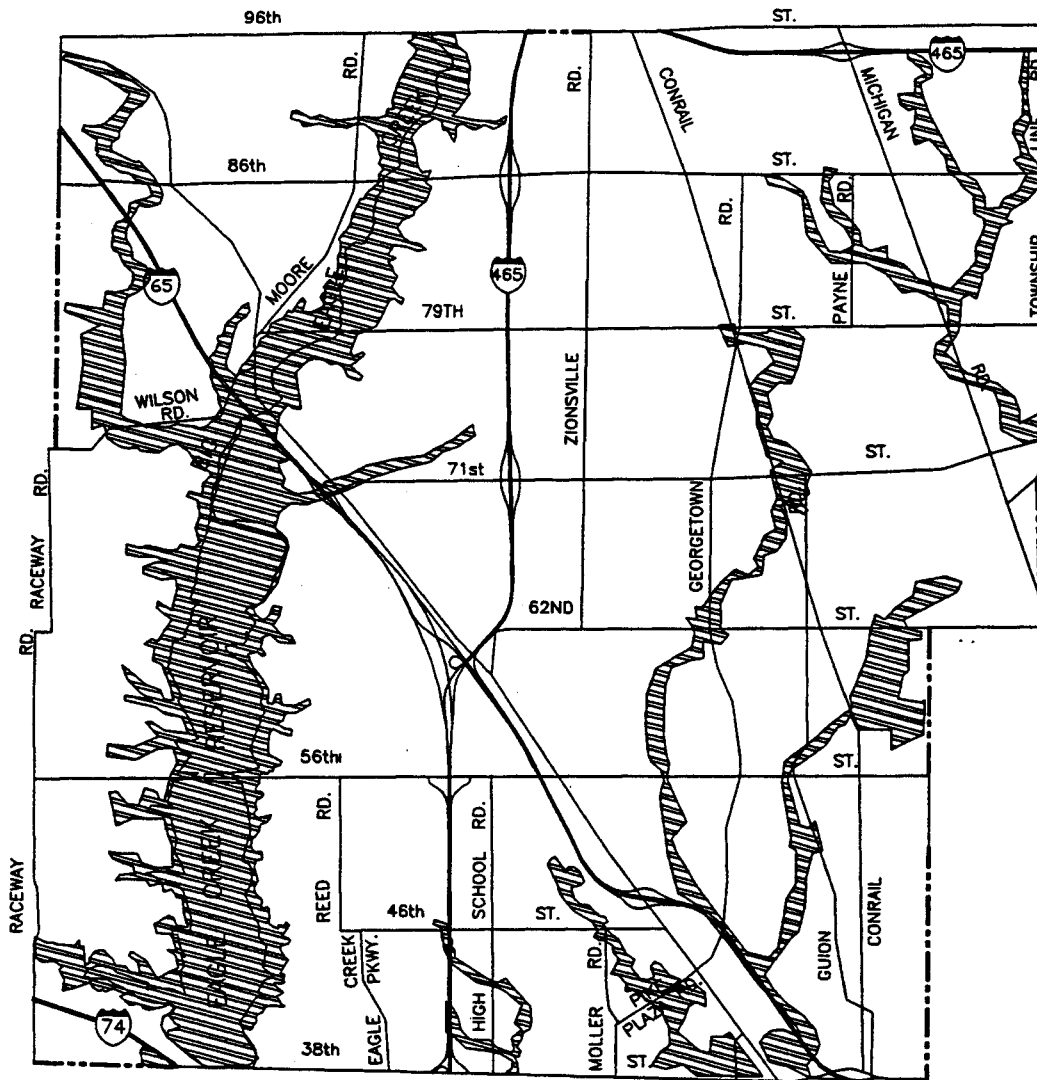
*** This category of Public/Semi-Public use includes indexed Urban Conservation.

**** The 1984 and 1991 Comprehensive Plans assume full development of the township, with no vacant or agricultural land.

- *Most of the more recent development has occurred in concentrated growth areas distributed throughout the township.*
- All land use categories have experienced recent growth. In fact, between 1980 and 1990, significant expansions of residential, commercial, and industrial development have occurred in Pike. While multi-family housing was a large contributor to the residential growth in the early portion of the decade, single-family residential growth has steadily continued. Several new housing developments are occurring in the greater northeastern quadrant and also in the southwestern portion of Pike township. Considering that many platted subdivisions are not yet fully developed, and that many sizeable tracts of undeveloped residentially zoned land remain in the area, a continuation of this trend is expected.
- Much of Pike Township's new commercial development has taken place in and near the College Park development in the northern part of Pike, and in the Eagle Highlands area located in the southwestern portion of the township, each proximate to the more rapidly developing residential areas. Significant commercial growth has also continued near the Lafayette Square commercial complex located in the southeastern quadrant. Smaller neighborhood and community serving commercial centers have developed along several of the arterial streets, the most significant being at the intersection of West 56th Street and Georgetown Road.
- Most industrial development is located in the north-central portion of the township, along the Conrail Railroad, and bordered by I-465 on the north and west sides. Substantial industrial growth and development in recent years has occurred in and around Park 100, the largest industrial park in the state. The Dow-Elanco venture, currently under construction in the northwest corner of this industrial area, will add significantly to the industrial base of Pike Township.

Comprehensive Plan Response - Industrial land use is recommended for much of the remaining area in and around Park 100. Continued commercial growth is anticipated as well, with new commercial centers planned for the land areas immediately adjacent to the intersections of major thoroughfares, such as Georgetown Road at 71st Street, Lafayette Road at 56th Street, and Marsh Road at 71st Street. Medium density residential development and office buffer areas are planned for areas near these commercial centers. Other future residential development should continue to be low or very low in overall density in order to minimize the impact on the area's infrastructure and to conform to the traditional character of the Township.

- *In much of the township, the roadway system is approaching its capacity.*
 - Although many roadway segments are currently operating below capacity, some of the key arterial streets (86th Street, 71st Street, 56th Street, 38th Street, and Michigan Road) are at or above capacity. Overall, approximately one-fifth of the township's roadways are projected to be operating at or above capacity levels by the year 2005.
 - *Comprehensive Plan Response* - Land use types and intensities appropriate to the projected future capacities of the infrastructure are recommended by the Plan. In addition, the Plan's policy recommendations include the stipulation that new developments should provide or make commitments for the provision of whatever additional infrastructure is needed to serve those developments. Plan recommendations for clustering of higher intensity uses proximate to one another at the intersections of arterial streets also works to minimize traffic congestion.
- *Compared to the rest of Marion County, Pike Township contains precious resources, including Eagle Creek Reservoir and Park, Big Eagle Creek, wetlands, many wooded areas, and rugged terrain.*
 - Water features traverse the entire township from where Big Eagle Creek enters the northern border of Pike from Boone County, and winds its way into Eagle Creek Reservoir which extends southward through the remaining two-thirds of the township. The reservoir, which currently provides millions of gallons of water each day to the public, and surrounding Eagle Creek Park provide nearly 3000 acres of natural beauty, recreation, and wildlife habitat to the citizens of Pike Township and Marion County.
 - Eagle Creek is prone to occasional flooding. Its floodplain (and those of its tributaries) contain significant wetland and woodland areas worthy of protection from destruction and elimination (see Maps 1, 2 and 3). Relatively little development has occurred in or near the stream corridor and floodplain, which increases the feasibility of establishing a linear park system, maintaining and utilizing the flood plain and wetland areas.
 - As in other areas of Marion County, where proposed development falls within the floodplain or floodway areas, the development regulations specified in the Flood Control Districts Zoning Ordinance apply.
 - Despite the high percentage of land devoted to park and open space uses (refer again to Table 1), Eagle Creek Park's substantial acreage skews the data, leading to potential misinterpretation. Excluding Eagle Creek Park, which is a regional park intended for use by all Marion County residents, Pike Township is under-served regarding park lands.



MAP 1

PIKE TOWNSHIP

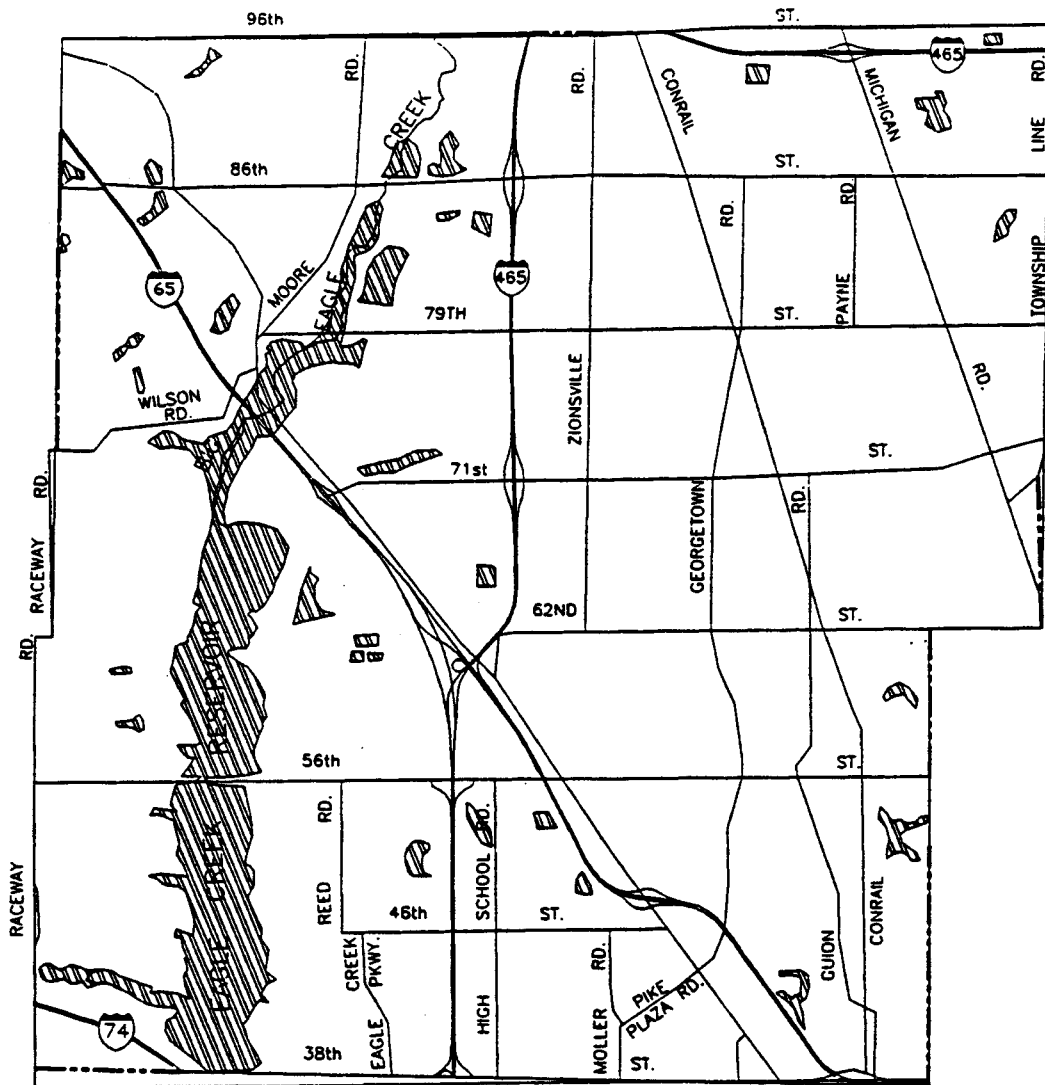
GENERALIZED FLOODPLAINS


FLOODWAY BOUNDARIES

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MAP 2

PIKE TOWNSHIP

GENERALIZED WETLANDS



WETLAND AREAS

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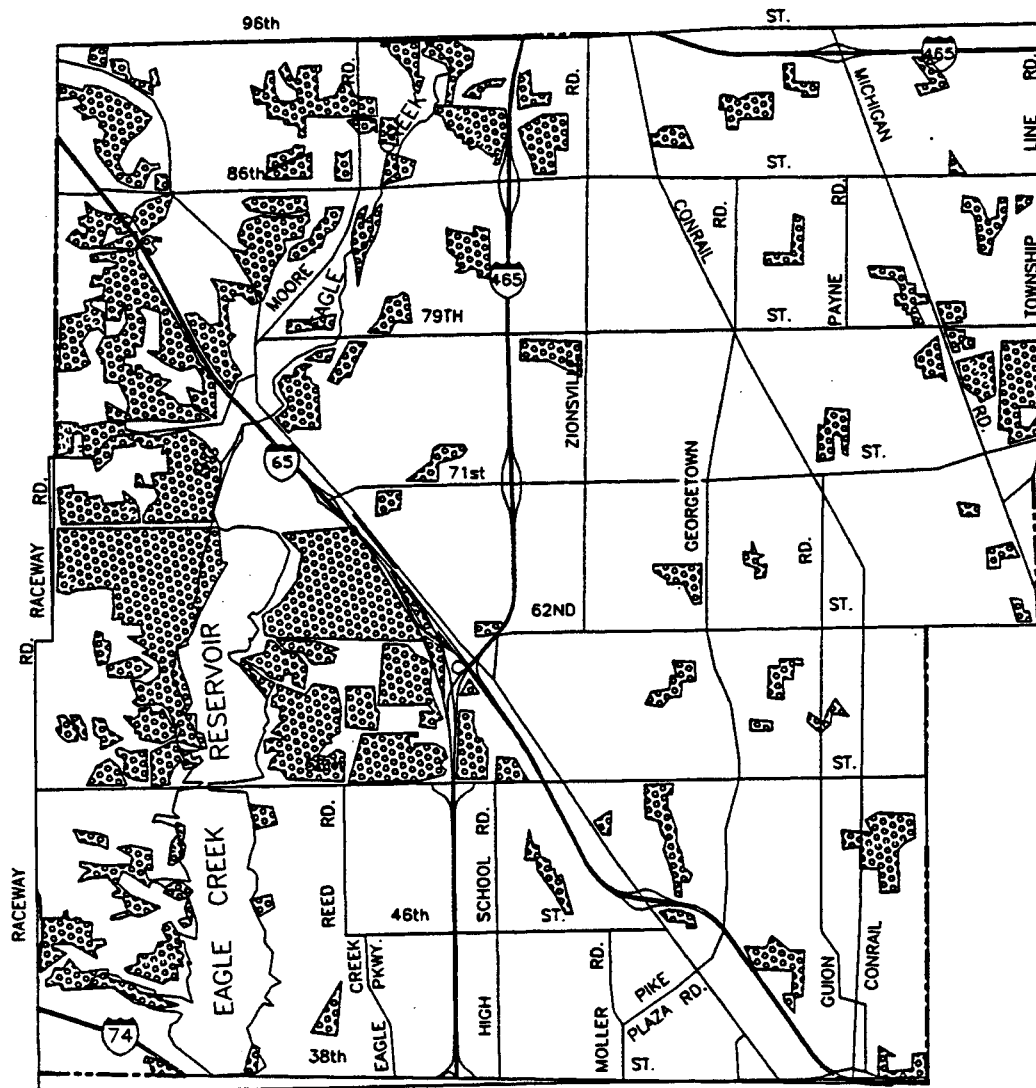
Comprehensive Plan Response - The Comprehensive Plan recommends establishing a linear park along Big Eagle Creek. The plan recommends other park uses, along with Urban Conservation and Very Low Density Residential development, adjacent to the floodways. The plan strongly discourages development of any kind in wetland and floodway areas. The plan also recommends the establishment of new smaller neighborhood and community parks to serve Pike Township's growing population.

- *Between 1973 and 1991, industrial land development increased at a rate greater than that of any other land use category.*

- Most industrial development is located near I-465 and West 86th Street in the north-central portion of Pike Township. Future industrial development is expected to occur in the same area. In fact, about one third of Park 100 has yet to be developed (approximately 500 acres). Another roughly 1500 acres of industrially zoned, vacant land lies outside of Park 100. Together sufficient land area exists in the Township to accommodate new industrial growth and expansions to existing industries well into the future.
- Commercial land uses in 1985 occupied 3.7% (1027 acres) of the developed land in Pike Township. By comparison, commercial uses occupied approximately 0.3% (71 acres) of developed land in Decatur Township and 0.6% (167 acres) of developed land in Franklin Township (in 1988.)

Comprehensive Plan Response - This Comprehensive Plan recommends that future commercial and industrial development occur near those areas currently developed for such uses, and primarily within those sites already industrially and commercially zoned.

Additionally, the Comprehensive Plan fully incorporates the recommendations and strategies for development contained in the *Michigan Road Corridor Plan* (adopted in April, 1988; see Map 4). That plan encourages controlled commercial development, elimination of physical and business deterioration, maintenance and expansion of housing stock surrounding the corridor, and the prevention of new and removal of existing industrial development in the area. The Corridor Plan also recommends improvements in transportation, public safety services, public facilities, and aesthetics of development in the corridor.



MAP 3
PIKE TOWNSHIP
GENERALIZED WOODLANDS



WOODED AREA

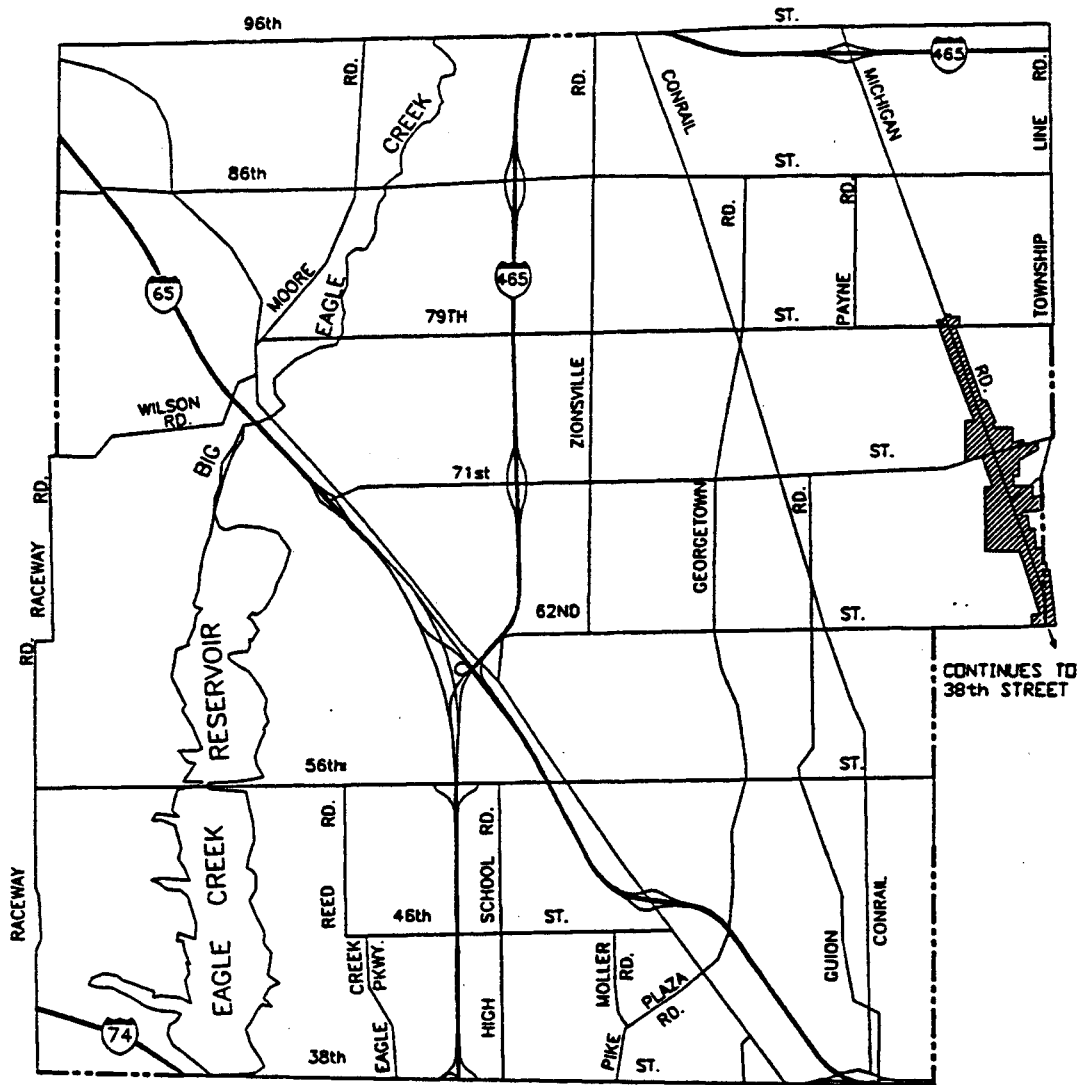
SOURCE: DIVISION OF PLANNING AERIAL PHOTOGRAPHS MARCH, 1991

NOTE: THIS MAP IS NOT EXHAUSTIVE. IT INCLUDES MOST WOODLANDS
OF TEN ACRES OR MORE.

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MAP 4

PIKE TOWNSHIP

MICHIGAN ROAD CORRIDOR PLAN AREA
IN PIKE TOWNSHIP



SUBAREA PLAN BOUNDARIES

(PLAN ADOPTED APRIL, 1988)

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Township Planning and Development Goals

The Pike Township Planning Committee (see Appendix A) identified the following goal statements to be reflected in the Pike Township Comprehensive Plan revision:

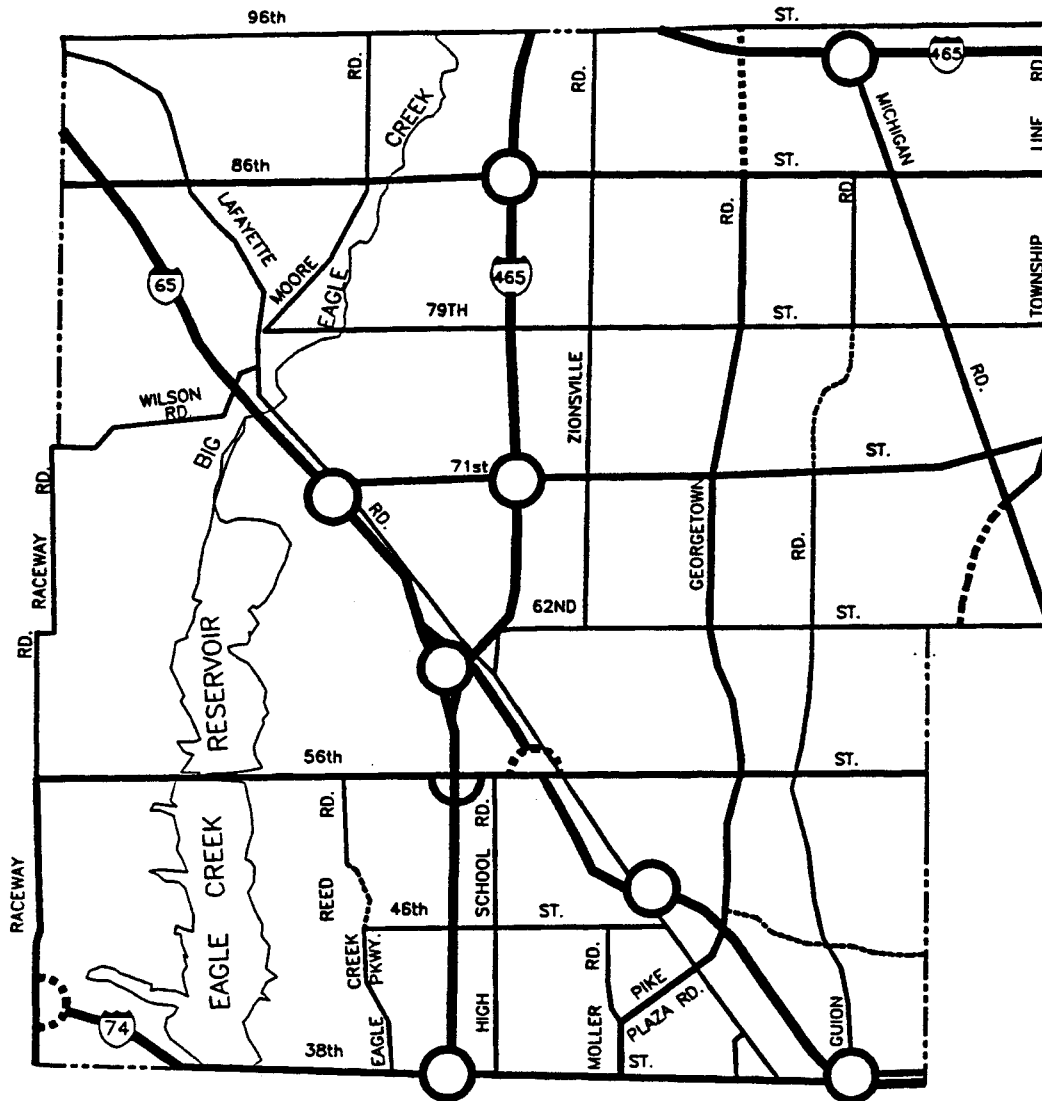
- Maintain a rural, estate residential development pattern in the extreme northwest;
- Encourage more ownership residential development and less rental residential development;
- Achieve development patterns and intensities which can be adequately served by the existing infrastructure systems;
- Promote / encourage phasing of development (to coincide with infrastructure improvements);
- Cluster residential development and maintain low overall densities in order to minimize potential conflicts with pipelines and environmentally sensitive areas;
- Discourage the development of lands necessary for safe landing and take-off operations at Eagle Creek Airpark;
- Limit industrial and larger-scale commercial activities to the area east of I-465, where infrastructure is already in place, and concentrate residential development, open space, and recreational uses in the area west of I-465;
- Control commercial growth along Michigan Road, West 86th Street, and 71st Street;
- Encourage integration of adjoining and neighboring commercial developments through the use of shared entrances, parking, signage, and pedestrian-friendly designs;
- Integrate fully the recommendations of the Marion County Comprehensive Parks plan;
- Construct more schools and establish additional neighborhood-serving park and open space areas to serve future population growth; and,
- Reduce dependency on Eagle Creek Park for everyday neighborhood recreation (Eagle Creek Park is a regional park).

Thoroughfare Plan

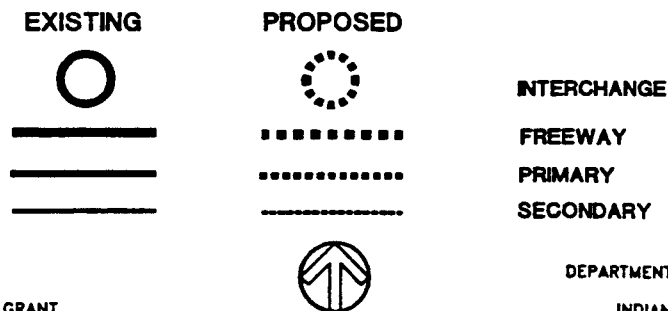
Pike Township is included in the Marion County Thoroughfare Plan. The Thoroughfare Plan recommends roadway improvements designed to mitigate congestion and delays, which are measures of the roadway system's efficiency (see Map 5). In addition, the Thoroughfare Plan includes recommendations for right-of-way preservation. The Thoroughfare Plan assigns each recommended improvement a priority rating indicating when the work should occur. Priority ratings range from "A" (highest priority) to "D" (lowest priority). The actual timing of implementation of the plan's recommendations is a function of available funding and the priority schedule for improvements throughout Marion County.

The Pike Township *Data Book* indicates that approximately 19% of the township's roadways are functioning near, at, or over capacity. By the year 2005, this number is projected to increase to 20% despite the assumption of having all priority improvements made. If the priority improvements are not made, a much greater percentage of the roadway segments would be operating near, at, or over capacity in 2005. To increase the efficiency of Pike Township's roadways and prevent their reaching or exceeding capacity in the future, the Thoroughfare Plan recommends the following priority improvements (see Map 6):

1. Widening of 86th Street from four lanes to six lanes between I-465 and Michigan Road (Priority A). [This improvement is partially complete, with the segment between I-465 and Zionsville Road having been constructed.]
2. Widening of Michigan Road from two lanes to four lanes between 62nd Street and 73rd Street (Priority A).
3. Widening of Georgetown Road from two lanes to four lanes between Lafayette Road and 56th Street (Priority A).
4. Widening of 46th Street from two lanes to four lanes between Moller Road and Lafayette Road (Priority A).
5. Widening of 38th Street from four lanes to six lanes between Lafayette Road and Kessler Boulevard, North Drive (Priority A). [The Pike Township portion extends from Lafayette Road to the eastern township line.]
6. Widening of 86th Street from two lanes to four lanes between Michigan Road and Township Line Road (Priority B).
7. Widening of Michigan Road from two lanes to four lanes between 79th Street and 86th Street (Priority B).
8. Widening of 71st Street from two lanes to four lanes between Guion Road and Michigan Street (Priority B).

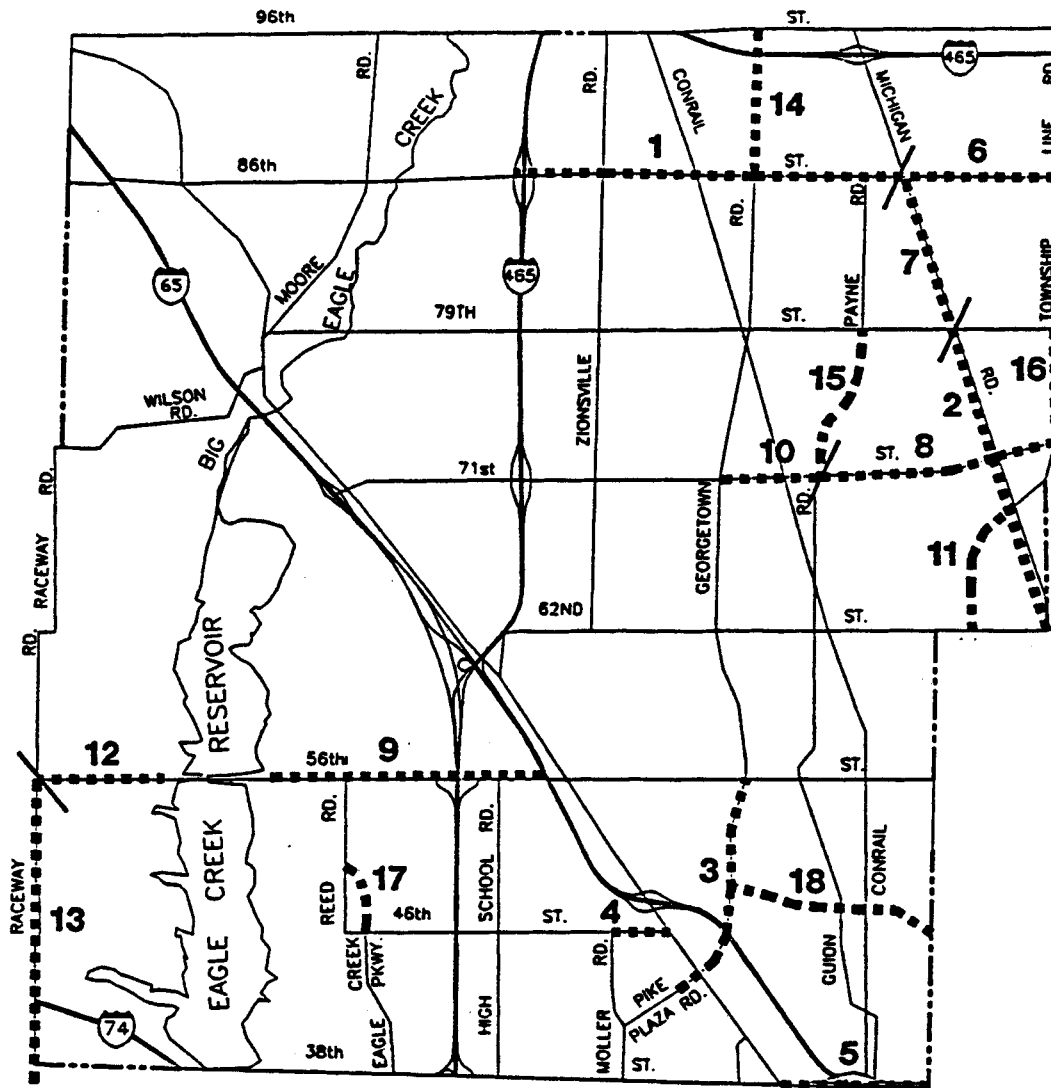


MAP 5
PIKE TOWNSHIP
PORTION OF THE OFFICIAL THOROUGHFARE
PLAN MAP, 1991



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MAP 6 **PIKE TOWNSHIP** **THOROUGHFARE PLAN PRIORITY IMPROVEMENTS**

PRIORITY A

1. 86th ST. WIDENING
2. MICHIGAN RD. WIDENING
3. GEORGETOWN RD. WIDENING
4. 46th ST. WIDENING
5. 38th ST. WIDENING

PRIORITY B

6. 86th ST. WIDENING
7. MICHIGAN RD. WIDENING
8. 71st ST. WIDENING
9. 56th ST. WIDENING

PRIORITY C

10. 71st ST. WIDENING
11. COOPER RD. CONNECTOR
12. 56th ST. WIDENING
13. RACEWAY RD. WIDENING

PRIORITY D

14. GEORGETOWN RD. EXTENSION
15. GUION RD. CONNECTOR
16. TOWNSHIP LINE RD. CONNECTOR
17. EAGLE CREEK PKWY. CONNECTOR
18. 46th ST. CONNECTOR

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9. Widening of 56th Street from two lanes to four lanes between Dandy Trail and Lafayette Road (Priority B).
10. Widening of 71st Street from two lanes to four lanes between Georgetown Road and Guion Road (Priority C).
11. Construction of a two lane secondary arterial connector of Cooper Road from 62nd Street to Lafayette Road (Priority C).*
12. Widening of 56th Street from two lanes to four lanes between Raceway Road and Bay Colony Lane (Priority C).
13. Widening of Raceway Road from two lanes to four lanes between Crawfordsville Road and 56th Street (Priority C). [The Pike Township portion extends from 56th Street south to the township boundary.]
14. Construction of a two lane secondary arterial extension of Georgetown Road from 86th Street to 96th Street (Priority C).*
15. Construction of a two lane secondary arterial connector of Guion Road to Payne Road from 71st Street to 79th Street (Priority D).*
16. Construction of a two lane secondary arterial connector of Township Line Road from 73rd Street to 79th Street (Priority D).*
17. Construction of a two lane secondary arterial connector of Eagle Creek Parkway from 46th Street to Reed Road (Priority D).
18. Construction of a two lane secondary arterial connector of 47th Street (46th Street) from Candletree Drive to Melbourne Road (Priority D).*

* The recommendation calls for an initial two lane construction. However, right-of-way should be preserved for an eventual four lane roadway.

The Pike Township land use recommendations are based in part on the Thoroughfare Plan's priority improvements and also on the levels-of-service of Pike Township's roadways.

"Level-of-service" is a measure of traffic congestion that rates roadways from A (least congested) through F (most congested). This measure identifies deficiencies in the roadway network. For example, a roadway segment with a level-of-service E or F is carrying more traffic than it is designed to carry.

The type and density of land use in an area determines the amount of traffic generated by that area. In turn, the amount of traffic generated affects roadway levels-of-service. Thus, the type and density of land use affects roadway levels-of-service. For example, traffic

generated per acre of development is usually higher for commercial centers than for residential uses, and higher for multi-family residential uses than for single-family residential uses. In general, the more traffic generated by a land use, the greater the effect on roadway levels-of-service. The land uses recommended in this plan reflect existing and projected roadway levels-of-service. Therefore, deviations from this plan's land use recommendations will inevitably alter projected roadway levels-of-service.

Stages of Development

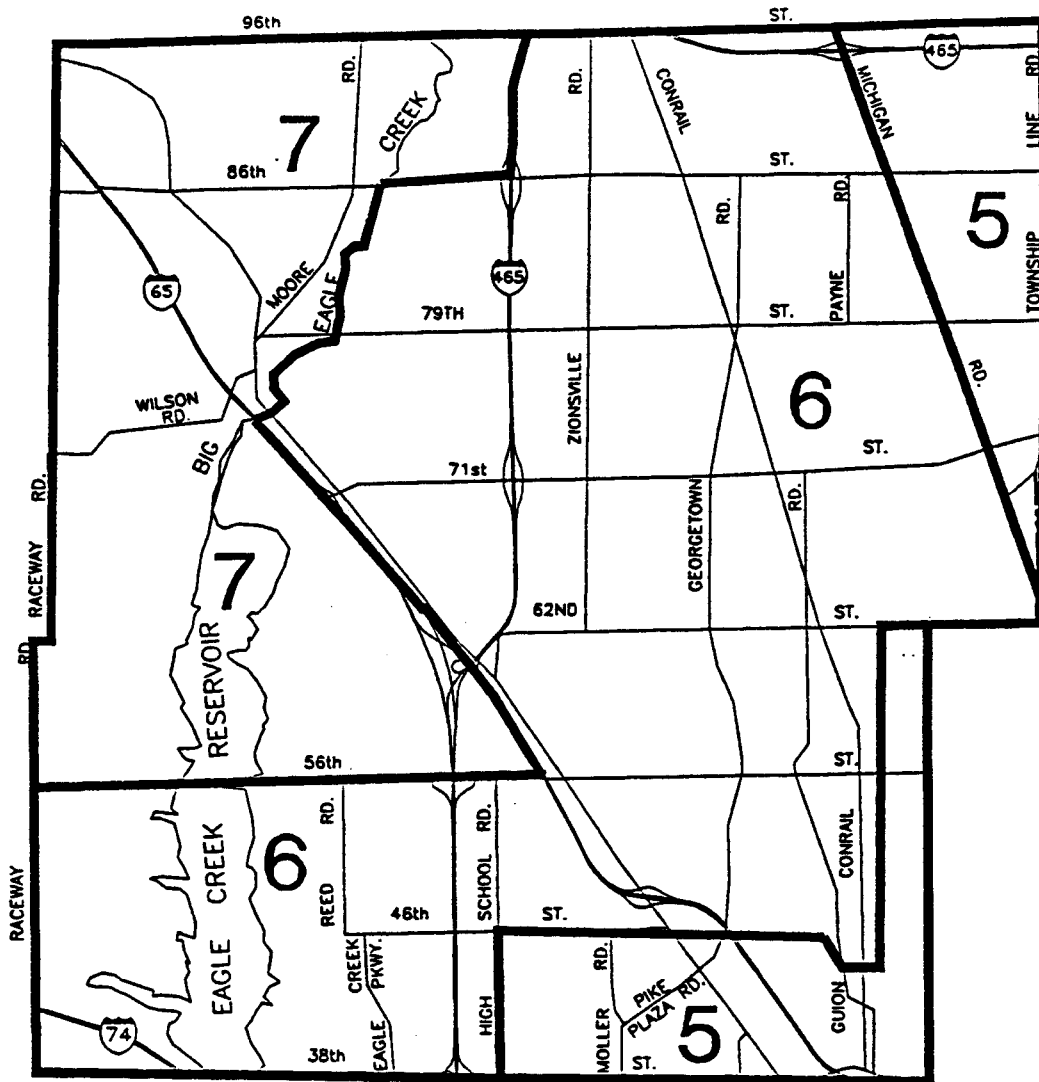
The Comprehensive Plan for Marion County differentiates areas of the county by their history, rate of development, and pressure for growth -- their stages of urban development. The entire county is comprised of stages numbered from Stage 1 which is the most intensely developed, to Stage 7 which contains the least amount of development.

Pike Township contains areas in Development Stages 5, 6, and 7. Stages 1 through 4, which represent older developed areas of the city, are not present in Pike Township. The location and development policies for each stage are as follows (see Map 7, and refer to the *Marion County Comprehensive Plan* narrative for additional information on Stages of Development):

- *Stage 1 (Regional Center Area) - Not present in Pike Township*
- *Stage 2 (Center City Revitalization Area) - Not Present in Pike Township*
- *Stage 3 (Established Center City Area) - Not Present in Pike Township*
- *Stage 4 (Suburban Revitalization Area) - Not Present in Pike Township*
- *Stage 5 (Established Suburban Area)*

Stage 5 presents itself in two areas of Pike Township: in the northeast corner (the area east of Michigan Road), and the southeastern corner (the area bounded by High School Rd. on the west and 52nd Street on the north, with a narrow extension on its east side reaching up to 62nd Street).

The Stage 5 areas in Pike Township are developed mostly for residential and commercial uses, although a small light industrial area exists in the extreme southeast portion of the second area. Few land parcels in the Stage 5 areas remain vacant, and they tend to be scattered. Infill development of these vacant parcels should be encouraged ahead of the development of more distant, unserved areas of the county, provided the land use is compatible with nearby existing land uses.



MAP 7

PIKE TOWNSHIP

STAGES OF DEVELOPMENT



STAGE BOUNDARY

5. ESTABLISHED SUBURBAN AREA

6. DEVELOPING SUBURBAN AREA

7. RURAL AREA

NOTE: STAGES 1 THROUGH 4 ARE NOT PRESENT IN PIKE TOWNSHIP

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The 71st Street/Michigan Road area in particular has recently exhibited characteristics of a Stage 4 (Suburban Revitalization) area. Long standing vacant commercial structures and deteriorating residential buildings are in need of investment and improvement. The policies identified in the Marion County Comprehensive Plan narrative regarding Stage 4 areas (see page 31 of that document) should apply to the area delineated as the *Michigan Road Corridor Plan* area on map 4 (page 10). A particularly important policy recommendation is that financial resources should be made available to assist in the financing of appropriate rehabilitation and reuse projects. Additionally, increased police presence is needed to ensure the public safety and to prevent an escalation of crime and vandalism.

■ *Stage 6 (Developing Suburban Area)*

Most of Pike Township has been identified as Stage 6. The Stage 6 area is generally south and east of Eagle Creek Park, and east of Big Eagle Creek.

Stage 6 represents the most recently developed and developing areas in the township. Although a greater percentage of its land is developed than in Stage 7 areas, Stage 6 areas in Pike Township contain large undeveloped tracts of land, especially between Georgetown Road and Michigan Road, and west of I-465.

Because much of the area in Stage 6 is served by or has access to water and sewer service, development pressure may exist for various potentially incompatible land uses on the remaining vacant areas. The Michigan Road, 86th Street, 71st Street, 56th Street, and Lafayette Road corridors are likely candidates for such development pressure. To ensure orderly growth and to prevent unmanaged and speculative land use decision-making, the boundary lines for recommended land uses -- particularly in designated critical areas -- should be interpreted as being definitive and fixed.

Significant and valuable environmental features such as the wetlands, wooded areas, and steep slopes should be conserved and protected from damaging or harmful development. Furthermore, development should be required to utilize buffering and other techniques which minimize potentially negative impacts upon nearby existing development. The Big Eagle Creek floodplain (and the floodplains of its tributaries) should not be developed. Open space should be preserved wherever extensive and/or intensive development is proposed. Such open space preservation will help to maintain the township's rural and suburban character, protect environmental features, and provide relief from the potentially monotonous effects of continuous urban development. Finally, all new development should occur with careful site design and impact planning, including the provision of adequate public facilities; such as park areas and infrastructure.

■ *Stage 7 (Rural Area)*

The northwestern remainder of the township is designated as Stage 7. Generally, less than 5% of Stage 7 land is developed, and that development is widely distributed throughout the area. The Stage 7 area in Pike Township consists primarily of rural farm land and large tract single-family "estate" development along existing roadways. There are very few platted subdivisions. Existing roadways are generally two lanes, with little or no system improvements. However, levels of service remain excellent due to low traffic demand. Other infrastructure systems, such as sanitary sewers and city water, are largely nonexistent.

The Stage 7 area includes Eagle Creek Park and a substantial portion of Eagle Creek Reservoir's tributary watershed. The Big Eagle Creek and Fishback Creek valleys are both significant features in this area, with each containing significant wetland and floodplain areas unsuitable for development. Because these streams flow into Eagle Creek Reservoir, a major water supply for the City and the most significant water recreation destination for the entire metropolitan Indianapolis community, it is important to ensure that the highest water quality be maintained. Therefore, commercial and industrial land uses, which could potentially contaminate areas of Eagle Creek Reservoir's contributing watershed, are not appropriate and are not planned. Rather, preserved woodlands and wetlands, open space, and very low density residential uses are the most appropriate for this area. To further minimize the potential for high storm water pollution levels, overall development densities in this area should be kept to an absolute minimum. Finally, cluster subdivisions and significant tree preservation commitments on the part of developers will also be necessary to ensure implementation of this objective.

The Stage 7 area is also characterized by hilly terrain which offers a unique topographic setting within Marion County's otherwise relatively flat and featureless plain. These hillsides, and the vistas they offer, are worth protecting as well. In order to minimize the potential for soil erosion on the hillsides and to preserve the vistas afforded by the natural topography, overall densities should be kept to a minimum and subdivisions should be clustered away from steep slopes.

To prevent overloading of roadway, sewer, and water system capacities in this area, require new development to:

1. Demonstrate the existence of sufficient reserve capacities in existing infrastructure systems to accommodate the additional demand; or
2. Identify and provide for the expansion of deficient infrastructure systems (roadways, sewers, etc.) concurrent with construction in order to ensure uncompromised service to all affected residents.

Overall, new development should occur in a manner which reflects and helps preserve this area's rural character. Generous setbacks from the existing arterial and collector roadways, clustered residential subdivisions, and substantial tree preservation commitments will be essential contributing factors to the successful implementation of the plan's land use recommendations and the Stage 7 objectives stated here. The clustering of homes and the maintenance of lower overall densities will also better accommodate areas which are constrained by the presence of pipelines (see Map 8).

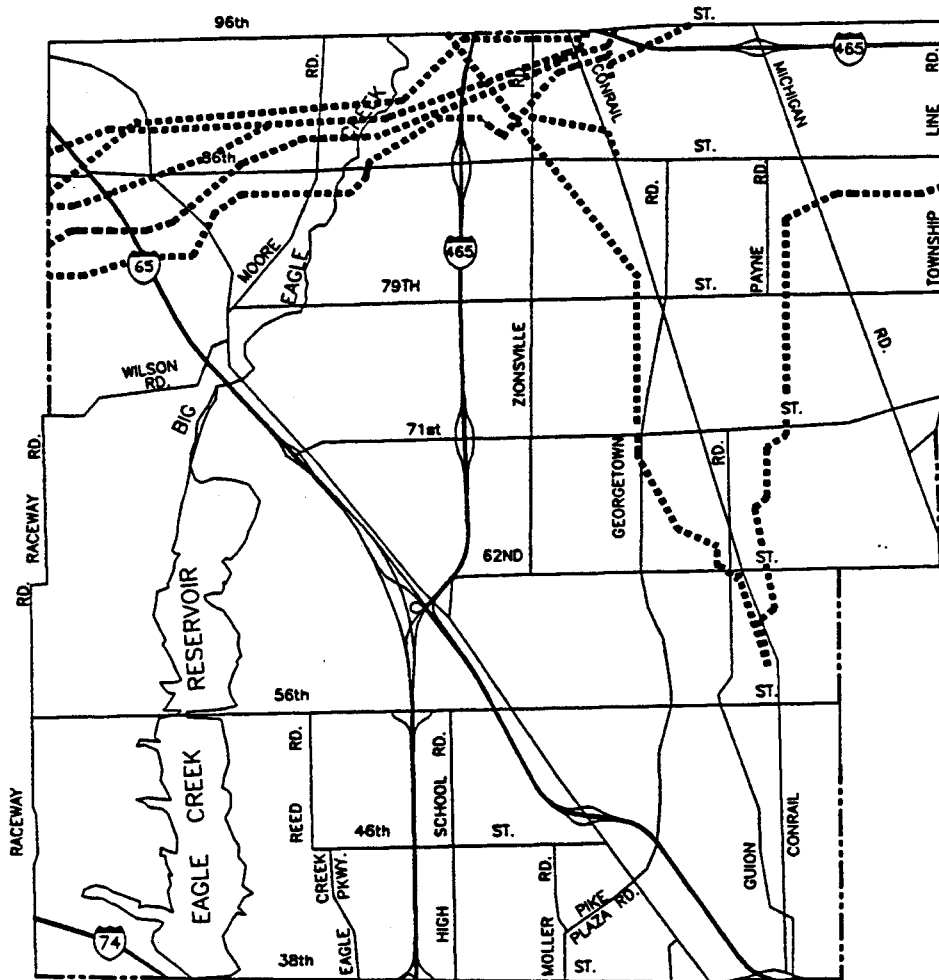
Critical Areas

Some land use recommendations in Pike Township warrant special emphasis because of factors related to their location, unusual character, and significant development potential. It is essential to the Pike Township Comprehensive Plan that these "critical areas" be developed as recommended. The plan's land use recommendations for the remainder of the township are also important, but critical areas warrant a more detailed explanation.

This list of critical areas can only be expanded by the adoption of a new critical area as a Comprehensive Plan Segment. This process involves further study of an area by the Department of Metropolitan Development, which may then recommend an area to the Metropolitan Development Commission for adoption as a Comprehensive Plan Segment.

The following are Pike Township's critical areas, including a description and map of each, the rationale for why it is designated "critical" to the plan, recommendations for development, and additional data (for all critical areas, see Map 9). Floodplains, wetlands, proposed Thoroughfare Plan roadway improvements, and other features are also shown on the individual critical area maps.

As stated in the introduction, this *Pike Township Comprehensive Land Use Plan* narrative is intended to be used with the *Marion County Comprehensive Plan* narrative, the *Marion County Thoroughfare Plan*, and the Pike Township Comprehensive Land Use Plan Map. The concepts contained in the *Marion County Comprehensive Plan* include land use categories, indexing, and policies for development. They apply throughout the county and are more fully explained in the *Marion County Comprehensive Plan*. Likewise, the *Marion County Thoroughfare Plan* contains more detailed information regarding roadways in Marion County. Consult these plans if necessary when using this Pike Township narrative.



MAP 8

PIKE TOWNSHIP

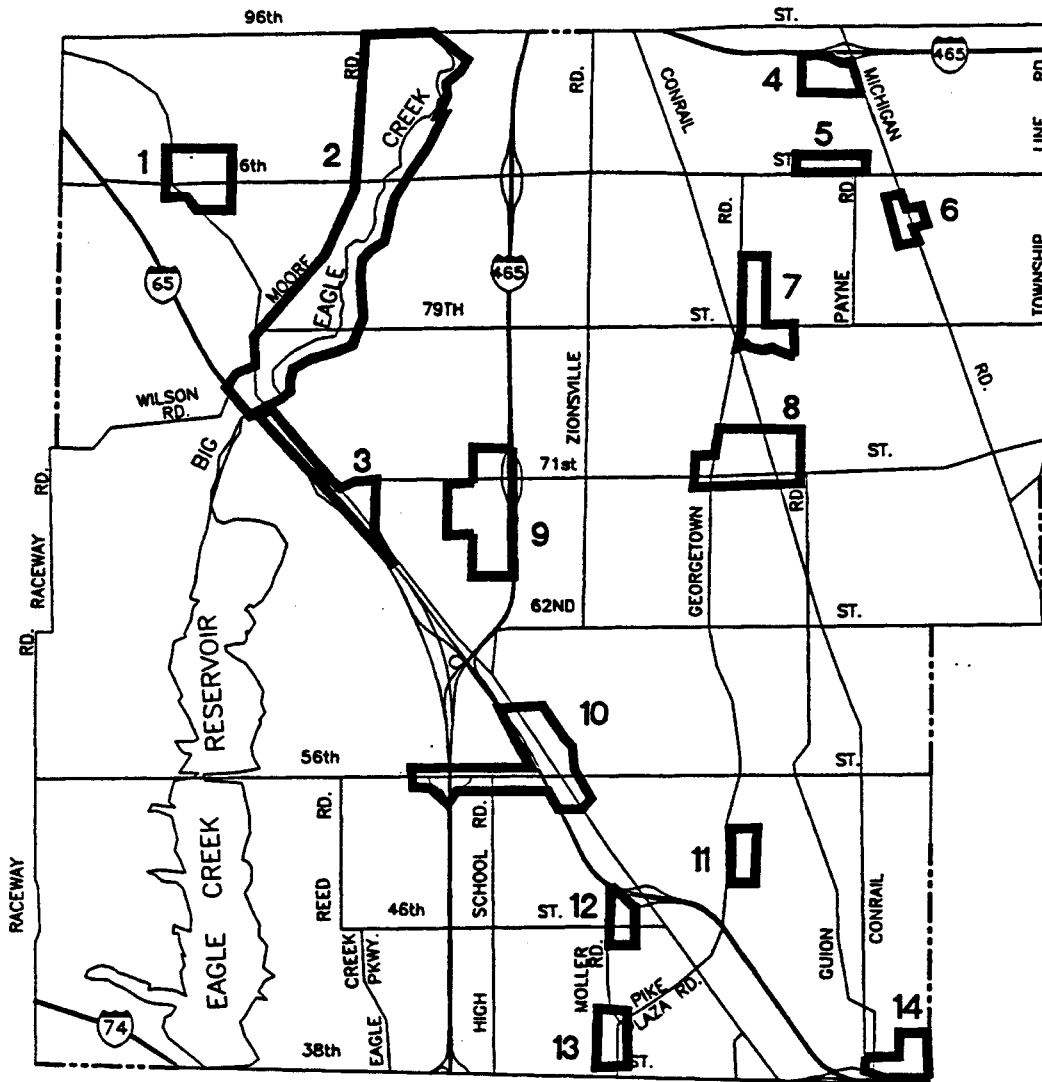
PIPELINES

..... SOURCE OF INFORMATION: 1980 TOPOGRAPHICAL MAPS.
PIPELINE LOCATIONS ARE APPROXIMATE.

THE PREPARATION OF THIS MAP
WAS FINANCED IN PART BY A
COMMUNITY DEVELOPMENT BLOCK GRANT



FEBRUARY, 1993
DEPARTMENT OF METROPOLITAN DEVELOPMENT
DIVISION OF PLANNING
INDIANAPOLIS-MARION COUNTY, INDIANA



MAP 9

PIKE TOWNSHIP

CRITICAL AREAS



CRITICAL AREA BOUNDARIES

THE PREPARATION OF THIS MAP
WAS FINANCED IN PART BY A
COMMUNITY DEVELOPMENT BLOCK GRANT



DECEMBER, 1992
DEPARTMENT OF METROPOLITAN DEVELOPMENT
DIVISION OF PLANNING
INDIANAPOLIS-MARION COUNTY, INDIANA

Critical Area 1

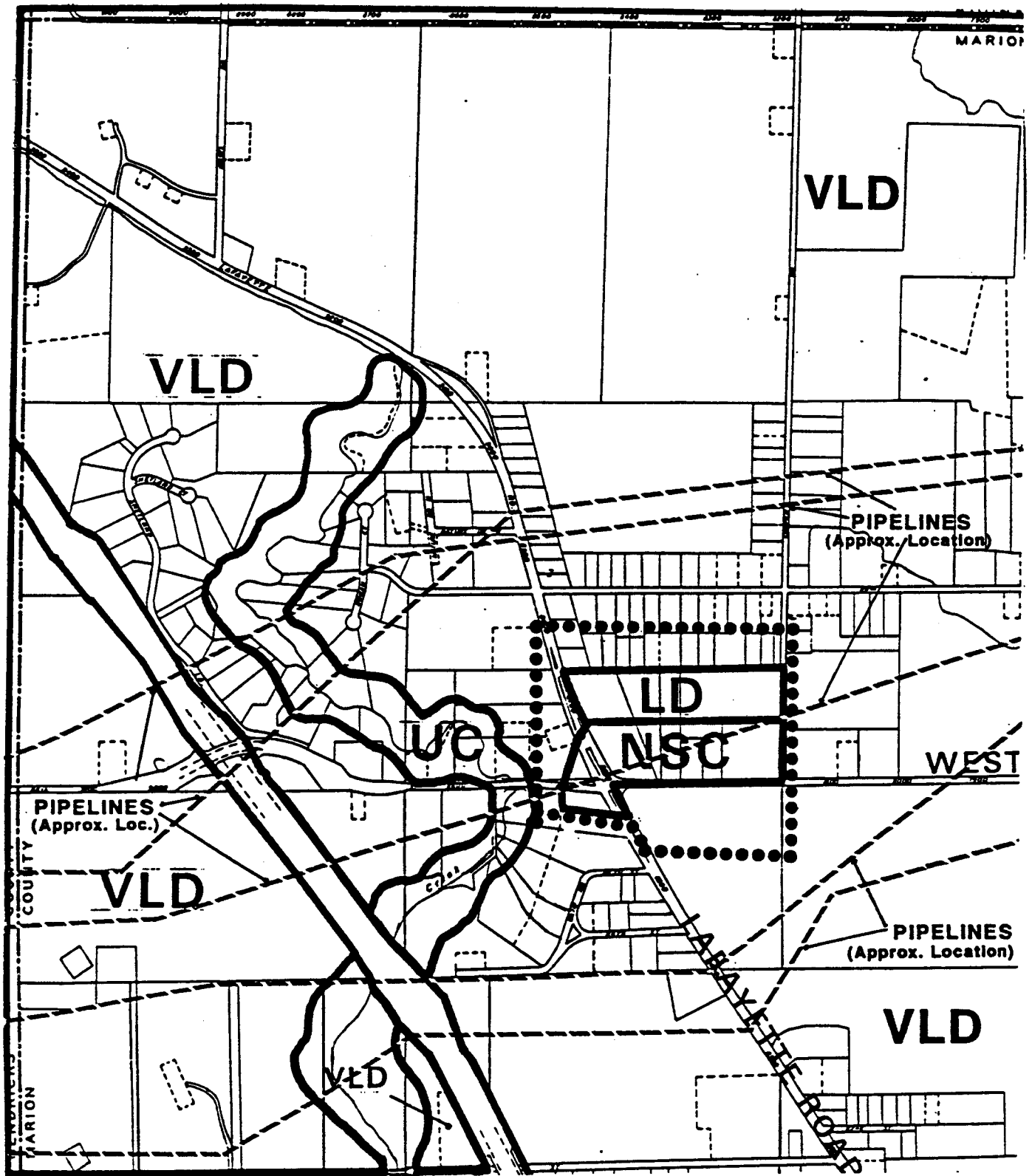
- Location (see Map 10): All four corners of land at the intersection of West 86th Street and Lafayette Road (approximately 48 acres).
- Land Use Plan Recommendations: Very Low Density Residential [VLD]; Low Density Residential [LD]; and Neighborhood Shopping Center [NSC]
- Surrounding Land Uses Recommended in the Comprehensive Plan:
 - North: Very Low Density Residential [VLD]
 - East: Very Low Density Residential [VLD]
 - South: Very Low Density Residential [VLD]
 - West: Very Low Density Residential [VLD]
- Stage of Development: 7 (Rural Area)
- Why Critical: This area already contains parcels zoned for retail commercial uses (C-3) and multi-family development (D-7), yet it is not served by sanitary sewer system or city water. Additionally, the surrounding area is characterized by large-lot and "estate" residential uses, as well as a future elementary school site at the intersection's southeast corner. If not done sensitively, future commercial and multi-family residential development in this area could adversely impact the surrounding properties.
- Recommendations:
 - a. Limit future commercial development to the area recommended on the Land Use Plan Map. Retail uses should be oriented towards serving the needs of the immediate neighborhood only, and the building (or buildings) should incorporate residential design characteristics (scale, adornments, materials, roof lines, etc.) as much as possible.
 - b. The area recommended for Low Density Residential (LD) on the Land Use Plan Map should be developed at a lower density than the maximum allowable under the existing D-7 zoning. The buildings should be well spaced across the property, and they should exhibit single-family home design characteristics as much as possible.

- c. Development of either the Neighborhood Shopping Center or the Low Density Residential area should not occur until municipal services (sanitary sewer and city water) are available.

■ Additional Data:

- a. Soil Limitations - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by significant shrink-swell properties, severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. Drainage - Drainage problems are commonly associated with the Crosby and Brookston soils present in this area. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - The western edge of the critical area is characterized by steep slopes. Because of their susceptibility to erosion, these steeply sloped hillsides should be protected from development. An underground pipeline traverses the critical area. The approximate location of it and several others nearby, are shown on Map 10. These locations should be taken into consideration with any proposed development.
- d. Sanitary Sewers - This area is not served by sanitary sewers. Existing development utilizes septic sewer systems. Because the area's soils are not well suited for septic systems, new development should not take place unless sanitary sewers are provided.
- e. Water Facilities - Water service is not yet available in this area.
- f. Transportation - The Official Thoroughfare Plan for Marion County recommends that 86th Street remain a primary arterial, and that Lafayette Road remain a secondary arterial.

Map 10 - Critical Area 1



Critical Area 2

- **Location (see Map 11a and 11b):** Both sides of Eagle Creek, from 96th Street (also the Boone County Line) on the north, to Eagle Creek Reservoir on the south (approximately 340 acres).
- **Land Use Plan Recommendations:** In the area generally following the Eagle Creek floodway; the primary recommendations are Linear Park (Proposed) (LP P), Community Park (Proposed) (CP P) and Urban Conservation (UC). In a few cases, these are accompanied by the indexed, or secondary, recommendation of Very Low Density Residential (1).¹
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** The Boone County Comprehensive Plan recommends residential use north of 96th Street. The specific area immediately north is in the flood plain.
 - East:** Urban Conservation indexed to Very Low Density Residential - UC (1)
Very Low Density Residential - VLD
Low Density Residential- LD
 - South:** Urban Conservation - UC
Urban Conservation indexed to Very Low Density Residential - UC (2)
Regional Park (Existing) - RP (E)
Very Low Density Residential - VLD
 - West:** Very Low Density Residential - VLD
- **Stage of Development:** 7 (Rural Area)
- **Why Critical:** Most of this critical area's 340 acres are wooded areas, wetlands and floodway, as Eagle Creek winds a north-south path through the center of the area. These woods and wetlands, along with the Eagle Creek floodway and its fringe area, make Critical Area 2 an environmentally sensitive natural area that merits close scrutiny.

In addition to the intrinsic value of the area's woodlands and wetlands as natural assets, these areas provide a natural habitat for various wildlife species. These natural

¹ Indexing is a method of numerically assigning a secondary land use recommendation to an area. On the Pike Township Comprehensive Plan map the indexed land use is shown in parentheses next to the primary land use recommendation. For further explanation of the indexing concept, please refer to the Marion County Comprehensive Plan narrative, pages 12-13.

features also help make the Eagle Creek floodway and its fringe area ideal for linear park use.

Another factor supporting this area's designation as a critical area is its potential ability to serve as a ground water resource. Indeed, this factor's importance will increase along with future growth, and water supply will become a critical issue in meeting the water demands which correspond with such growth.

This valley, upstream of Eagle Creek Reservoir, contains alluvial sand and gravel deposits which bear northwestern Marion County's only significant source of ground water. Therefore, the natural features of the area, the aquifer system associated with the Eagle Creek Valley, as well as the creek itself, combine to warrant this region's critical area designation.

■ **Recommendations:**

- a. Protect the area's woodlands through tree preservation. Tree preservation is a community-wide objective, and this area contains numerous parcels of woodlands stretching from Eagle Creek Reservoir to the Marion County Line. Trees help prevent soil erosion, enhance the area's natural beauty, filter pollution, mitigate the higher temperatures associated with paved surfaces, and provide a necessary habitat for various wildlife species.
- b. Protect the area's wetlands. Wetland areas benefit the entire community: they provide natural retention areas for floodwater, natural filtration systems for storm water runoff, and habitats for various wildlife species. The wetlands depicted on the Critical Area 2 map are generalized from the National Wetlands Inventory Map. More detailed wetland information is available from the U.S. Department of the Interior, Fish and Wildlife Service and the Marion County Soil and Water Conservation District.
- c. Create a linear park along Eagle Creek. (A linear park is a connected system of landholdings creating a chain of open space.) The Comprehensive Plan recommends that a linear park be established between 79th Street and the proposed Community Park, to be located just south of 96th Street. The linear park would help serve the area's recreational needs. It would also make excellent use of the natural features as it preserves them. To meet these objectives, the linear park should serve passive recreational activities such as biking, running, and hiking.
- d. Protect the aquifer, which is the only significant source of ground water in the northwestern portion of Marion County. In the future, it may provide drinking water to much of the northwest metropolitan area (refer also to recommendations e through p, below).

- e. Keep development densities low, preserving as much natural open space as possible. Where recommended park and Urban Conservation uses are indexed to Very Low Density Residential development, limit gross residential density to no more than one unit per acre and use clustering techniques (see Recommendation n, below). Minimizing impervious surface area will reduce the chance of flooding and allow more rainwater to reach the aquifer.
- f. Restrict uses to only those which pose no threat of contamination to Eagle Creek or the associated aquifer, underlying the area. Prohibit the placement of threatening land uses. For example, dry cleaning establishments, car washes, chemical storage, and gasoline service stations are *not* acceptable uses in this critical area. The permanent storage and regular use of chemicals on site make such businesses potentially harmful to the drinking water supply. All development petitions must be evaluated carefully by staff regarding the use or storage of petroleum products, detergents and other commercial cleaning agents, salts, solvents, and other chemicals. Development petitions must include appropriate commitments or conditions that prohibit unacceptable uses. The Indianapolis Water Company must also be consulted during the development review process.
- g. Develop an "Aquifer and Well Field Protection Plan" that would summarize existing protection programs, map primary and secondary well field protection areas, and recommend detailed land use and development policies. Such a plan should include an inventory of past, present, and potential sources of groundwater contamination and a contamination contingency plan. The Marion County Health Department is an appropriate agency to undertake the plan.
- h. Prohibit dry wells in this critical area. Most drainage measures allow storm water to slowly percolate through the soil layers, be cleansed of some contaminants and eventually reach the aquifer. Dry wells, on the other hand, facilitate site drainage by bypassing layers of soil, and consequently they more directly expose the aquifer to contaminants.
- i. Educate property owners and lease holders to prevent groundwater contamination through proper management of the application of chemicals on agricultural, residential, golf course, and park land. The Indianapolis Water Company, Marion County Health and Hospital Corporation, and the Department of Metropolitan Development can conduct public education seminars, advertising campaigns, or neighborhood meetings to encourage conservation and protection of this aquifer and others in Marion County. Such conservation methods may include organic or other non-chemical means of controlling insects and "Tox-Away" programs for safe disposal of toxic household wastes.

- j. Post signs along 79th Street, 86th Street, 96th Street, Noel Road and Moore Road which display emergency phone numbers to facilitate immediate notification of fire departments and other emergency personnel about potential groundwater contamination.
- k. Study the possibility of introducing an "Aquifer Overlay Zoning District" which could specify appropriate land uses, list prohibited chemicals, and provide other viable regulatory measures. Such a zoning district could be appropriate because of the importance of maintaining a safe water supply. Zoning district regulations may include but are not limited to the following:
- prohibition of land uses potentially threatening to groundwater;
 - transfer of development rights from this zoning district to an area outside of the aquifer area;
 - performance standards that set forth special requirements for potential developers in this area;
 - more stringent site plan review;
 - more stringent drainage standards to protect groundwater from contaminated storm water;
 - encouragement of cluster subdivisions;
 - impervious coverage standards to limit the area of streets and other impervious surfaces that prevent water from entering the ground;
 - chemical and fuel storage restrictions;
 - stringent sewage discharge permits;
 - regulation of septic system cleaners;
 - conversion of septic systems to sewer systems; and
 - nitrogen loading standards to control the amount of nitrogen that is added to the soil via fertilizers.
- l. Utilize or create a foundation or other not-for-profit organization to obtain parts of the area for development as a park to ensure that valuable natural resources are preserved. Include in the foundation's membership government agencies, the Indianapolis Water Company, affected property owners and area residents, and environmental preservation organizations such as the Indianapolis Parks Foundation and Groups Advocating Urban Greenspace Environments (GAUGE). Purchasing the land for conservation as a wildlife refuge would offer the highest degree of protection for the aquifer and for wetland and woodland areas.
- m. In areas with indexed recommendations for Very Low Density Residential development, require potential developers to prepare and implement plans of operation which recognize and protect the aquifer. Include in the plans of operation guidelines for the prevention and abatement of groundwater contamination.

- n. In areas with indexed recommendations for Very Low Density Residential development, implement cluster development to help preserve environmentally sensitive areas. Clustering reduces the area of impervious street surface, allowing more storm water to be absorbed by the soil and ultimately return to the aquifer. In order to preserve wooded areas in cluster developments, require tree inventories and preservation plans.
- o. To preserve subsurface water quality in areas with indexed recommendations for Very Low Density Residential development, require all development to be served by sanitary sewers and city water. The development cost can be reduced through clustering (see Recommendation n).
- p. Closely monitor area development and its effect on groundwater quality. This should be done by the Indianapolis Water Company.
- q. Acquire right-of-way to meet the Thoroughfare Plan requirements, including any proposed realignment or widening of Streets.

■ **Additional Data:**

- a. **Soil Limitations** - The principal soils located within the area are Genesee silt loam , Martinsville silt loam and Miami complex. Genesee and Miami soils carry severe restrictions for building because of flooding and wetness. These characteristics are unfavorable for buildings, including residential structures. Because of the danger to Eagle Creek and the aquifer, septic sewage systems should not be allowed in this area.
- b. **Drainage** - Genesee silt loam, and Miami soils pose drainage problems for development in this critical area. Should any petitions for development arise in this critical area, they must include storm sewers as protection for the aquifer. This critical area contains significant wetlands, which help reduce the severity of floods by slowing and storing storm water. In addition, a large portion of the critical area is in the Eagle Creek floodway and floodway fringe. Potential development must be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements contained in the City Drainage and Sediment Control Ordinance.

Drainage in the Eagle Creek Valley is also affected by development in the Eagle Creek watershed north of Marion County. The City of Indianapolis should inform Boone County and Eagle Township officials of the drainage issues in the Marion County portion of the Eagle Creek Valley.

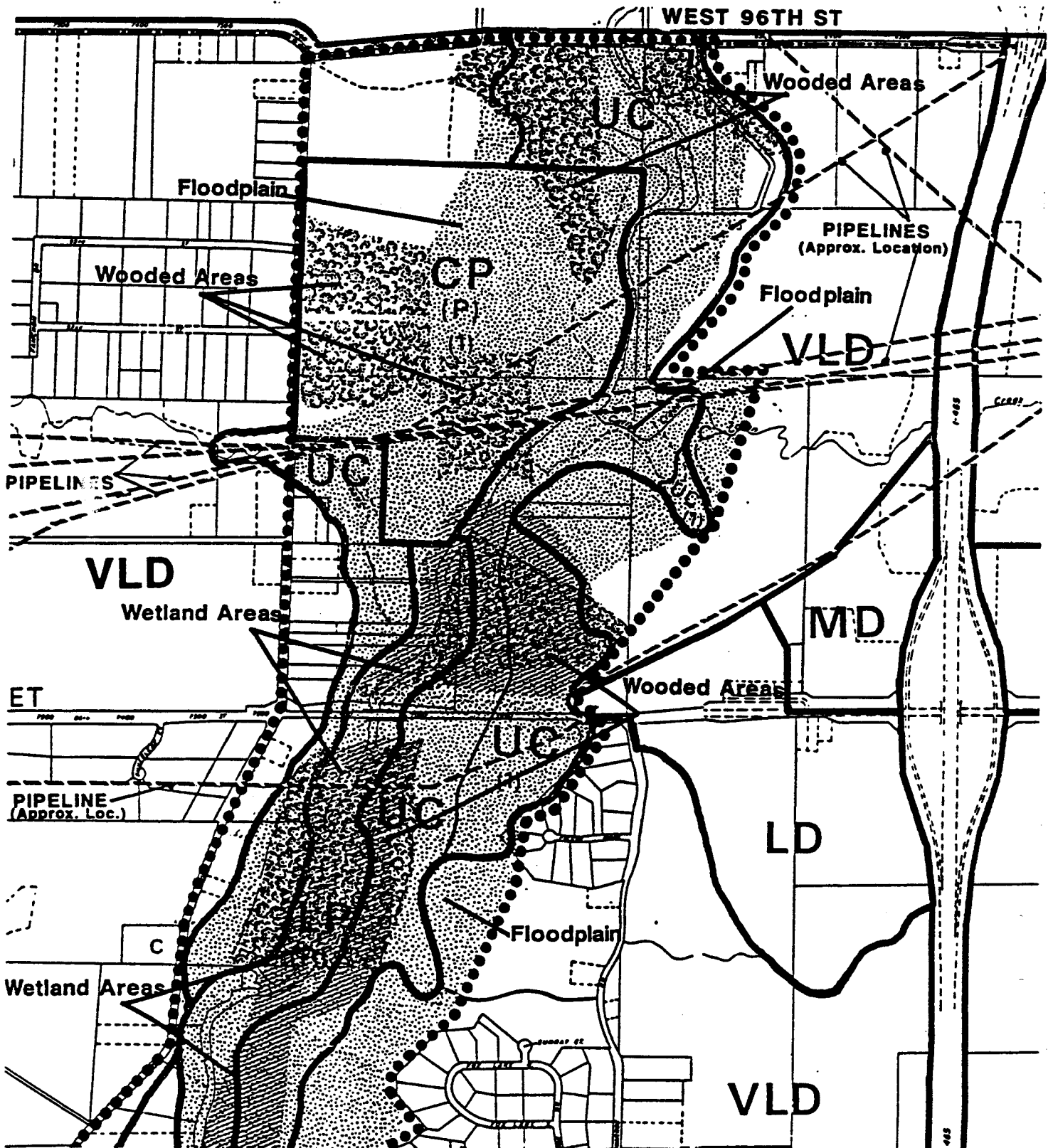
- c. **Environmental Constraints** - Any development must be sensitive to the aquifer flowing beneath Eagle Creek. In addition, partly due to their contributory impact on the aquifer's quality and long-term viability, and partly due to their own environmental significance, the existing wetlands, woodlands, and floodplain areas must be preserved to the greatest extent possible. The Linear Park, Community Park and Urban Conservation recommendations apply to areas that are steeply sloped, wooded, identified as wetlands, and/or are in the floodway of Eagle Creek. This area is an important component of the Marion County linear park system. The proposed Community and Linear Parks in Critical Area 2 are rated as having "very high" potential for a park site in the Comprehensive Parks, Recreation, and Open Space Plan for Marion County.

Several underground pipelines traverse this Critical Area. Monitoring wells should be placed strategically around and near these pipelines to detect any leakage into the subsurface water supply. Additionally, as these pipelines are maintained or rebuilt, protective spill containment basins should be constructed around each line to guard against the possibility of massive soil and water contamination resulting from a major break or leak.

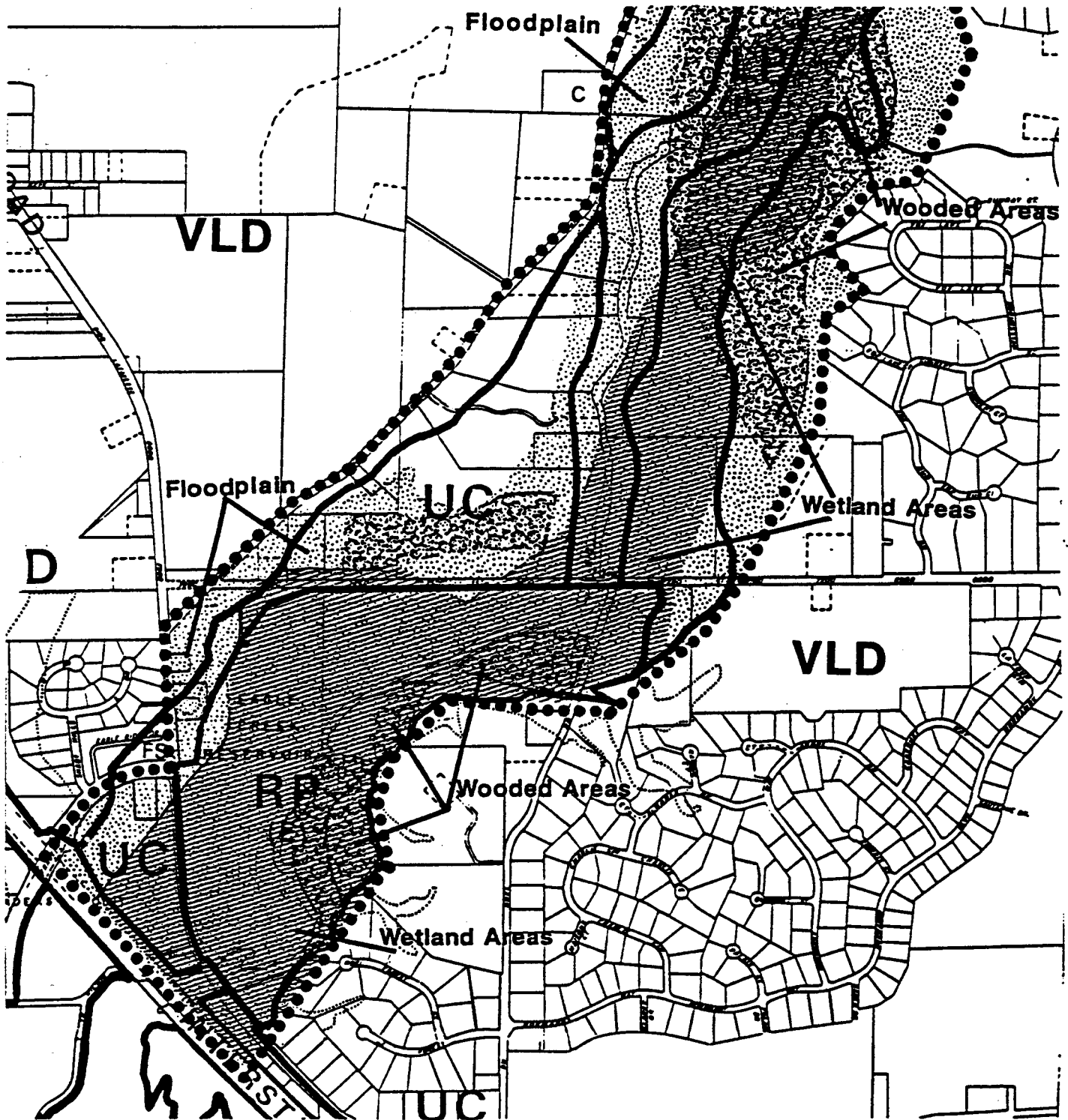
The use of pesticides, herbicides, fertilizers, and the storage and disposal of commercial and household wastes (e.g., automobile oil, cleaning products, paint, and other wastes) must be controlled within the critical area to prevent contamination of the aquifer.

- d. **Sanitary Sewers** - Only a small part of this area is currently served by sanitary sewers. The wetness and slow absorption of the predominant soil types severely restrict the effectiveness of septic systems. New development must provide sewer services to prevent health hazards associated with soils' inability to accommodate septic systems. Existing development should acquire sewers through the Barrett Law or another procedure. Potential developers should contact the Department of Public Works' sewer permitting division regarding sewer service needs.
- e. **Water Facilities** - Water is available at 86th Street. To accommodate new residential subdivisions in the area, a water main was recently extended west along 86th Street to a point approximately 2500 feet west of Eagle Creek.
- f. **Transportation** - The Official Thoroughfare Plan recommends that 86th Street be improved to a four-lane primary arterial.

Map 11a - Critical Area 2, Northern Section



Map 11b - Critical Area 2, Southern Section



Critical Area 3

- **Location (see Map 12):** East of Interstate 65; West of Lafayette Road, on the north side of 71st Street; both sides of Lafayette Road south of 71st Street (approximately 32 acres).
- **Land Use Plan Recommendations:** Urban Conservation indexed to Low Density Residential [UC(2)], and Very Low Density Residential [VLD].
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Very Low Density Residential [VLD]
 - East:** Very Low Density Residential [VLD]
 - South:** Interstate I-65, and Regional Park - Existing [RP(E)]
 - West:** Interstate I-65, and Regional Park - Existing [RP(E)]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** This critical area fronts on an interstate (I-65), with immediate access to it at the 71st Street interchange. However, unlike most interchanges in Marion County, this one does not serve any major population, employment, or commercial centers. Rather, it provides convenient access to Eagle Creek Park, the largest city-owned park in the nation. Any commercial uses or other intense development of land surrounding this interchange could severely impact the tranquil natural setting embodied within Eagle Creek Park.
- **Recommendations:**
 - a. Preserve as much open space as possible in the recommended Urban Conservation areas.
 - b. Carefully review development proposals for the undeveloped areas west of Lafayette Road due to their frontage on I-65 and the lack of substantial depth. Drainage from this area is into Fishback Creek and Eagle Creek; consequently, development of land within this critical area should result in no excessive storm water runoff, and minimal opportunity for stormwater pollution.

- c. The existing Indiana Department of Transportation chemical storage and vehicle maintenance facility located northwest of the 71st Street and Lafayette Road intersection should be relocated from this site to a less sensitive site, preferably in an industrial area. If the INDOT facility remains on this site, it should maintain all road de-icing chemicals under roof to prevent potential contamination of stormwater run-off and groundwater. Additionally, vehicles stored on the site should be kept along the western perimeter of the site, away from existing residences to the east. The eastern perimeter of the site should be thoroughly landscaped to screen nearby residential uses from the facility's industrial character.
- d. Commercial and other high intensity uses are not appropriate for this area. Future development should follow the land use recommendation of the Land Use Plan Map, with densities slightly higher than those of existing residential subdivisions in the area. Because of the close proximity of the interstate ramps to the Lafayette Road/71st Street intersection, the roadways in this critical area will not adequately carry the high traffic volumes generated by commercial uses.

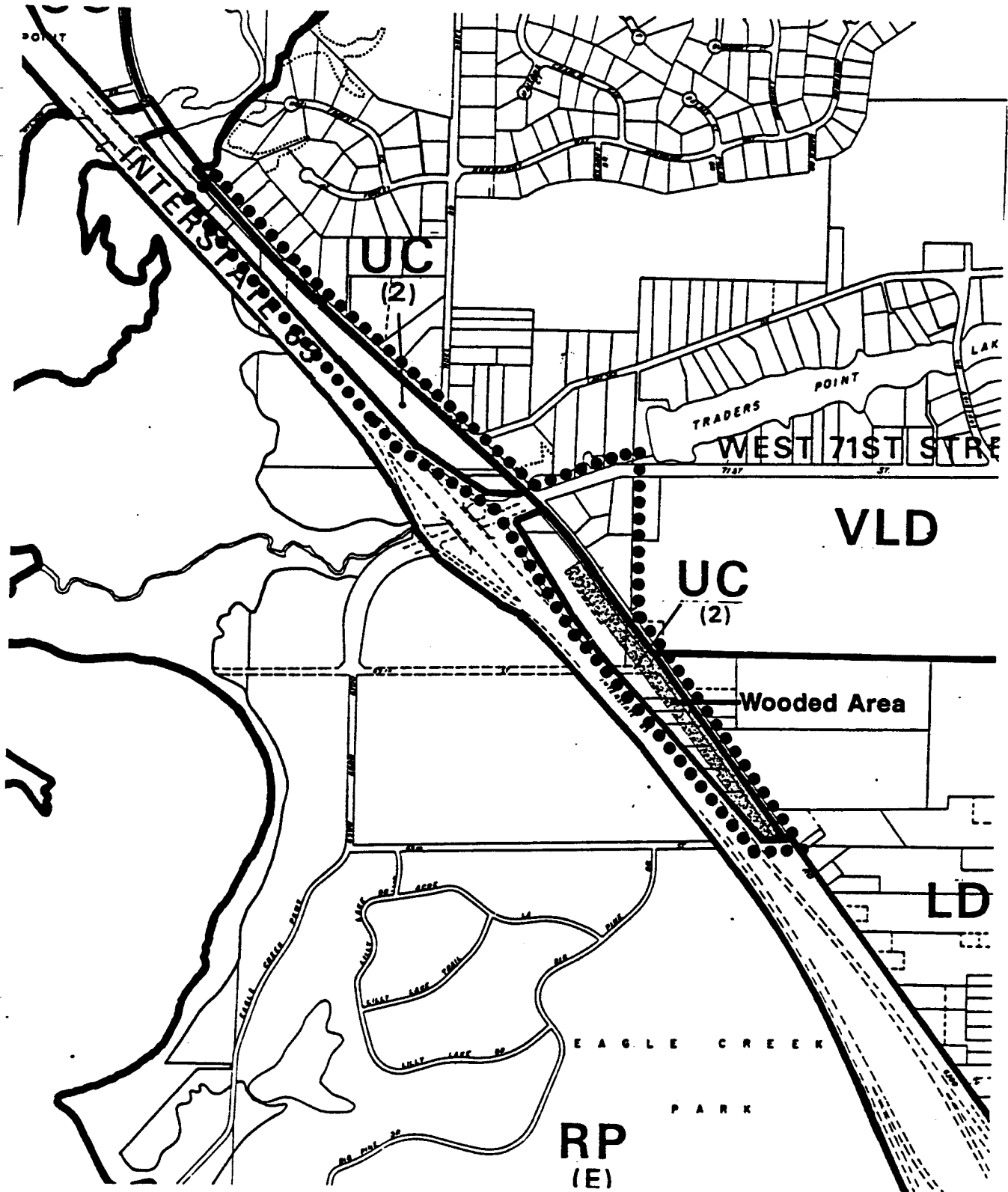
Low density residential use is recommended for the areas west of Lafayette Road via indexing. Low Density residential is appropriate because of the proximity of existing very low density residential development and of Eagle Creek Park. To maintain an overall density within the recommended range (2 - 5 units per acre), and to maximize the preservation of open space, cluster development should be employed.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Genesee and Miami Silt Loam soils. The tendency for water percolation to be slow in Miami soils, and the susceptibility to flooding in Genesee soils severely limit its development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area. Additionally, due to the poor strength of the Miami soils, intensity of development should be kept low.
- b. **Drainage** - No significant drainage problems are associated with this area. However, potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.

- c. **Environmental Constraints** - The northern end of the critical area is susceptible to flooding and includes steep slopes; consequently, it should be protected from development. The potential for contamination of surface water runoff from the State DOT facility should be minimized by storing all de-icing chemicals under roof.
- d. **Sanitary Sewers** - This area is not served by sanitary sewers. Existing development utilizes septic sewer systems. Because the area's soils are not well suited for septic systems, new development should not take place unless sanitary sewers are provided.
- e. **Water Facilities** - Water service is available via a water main along West 71st Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that 71st Street remain a primary arterial, and that Lafayette Road remain a secondary arterial.

Map 12 - Critical Area 3



Critical Area 4

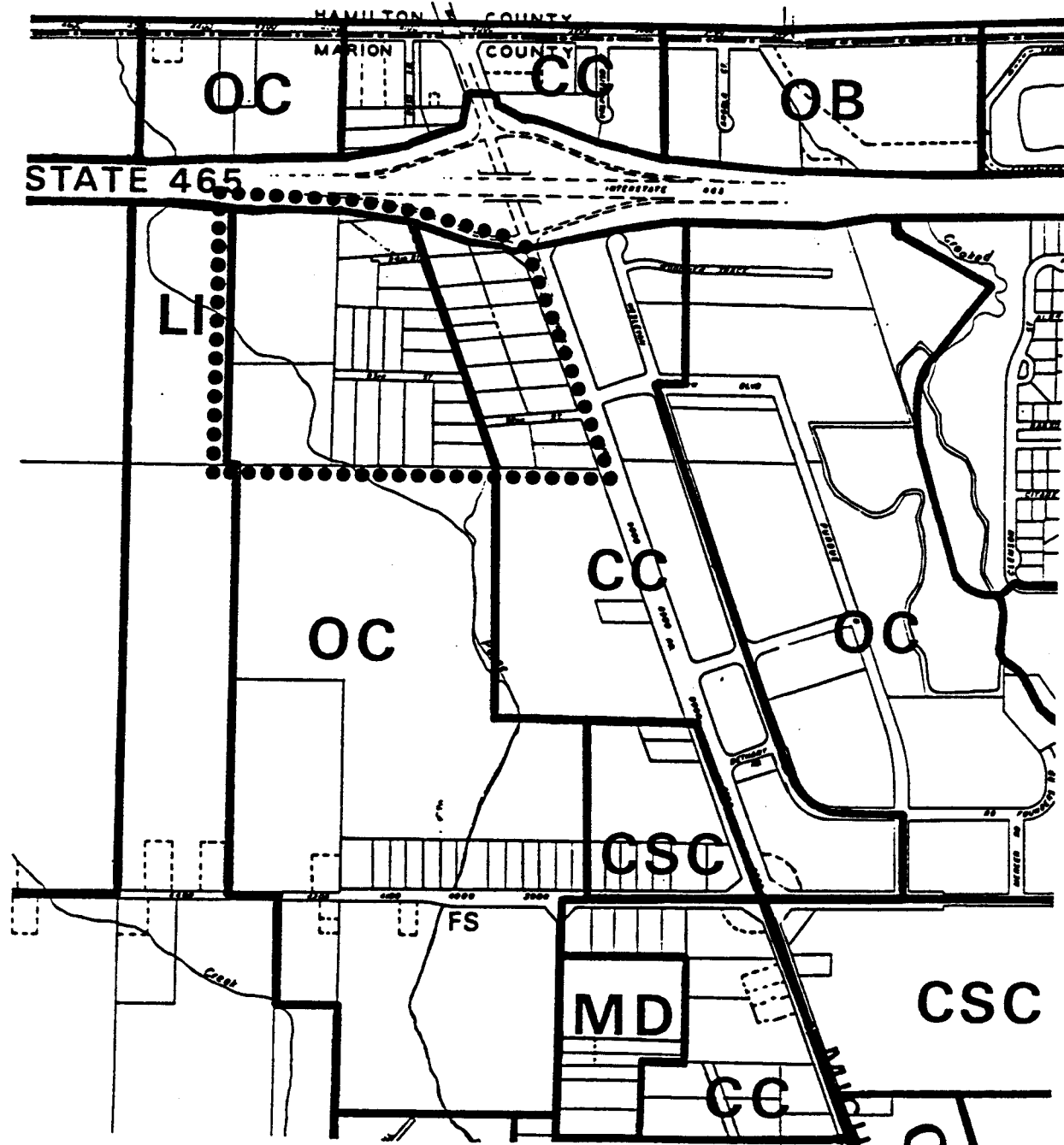
- **Location (see Map 13):** South of Interstate I-465, and West of Michigan Road (Approximately 68 acres)
- **Land Use Plan Recommendations:** Office Center [OC] and Commercial Cluster [CC].
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North: Interstate I-465
 - East: Commercial Cluster [CC]
 - South: Office Center [OC] and Commercial Cluster [CC]
 - West: Light Industrial [LI]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** Much of this area was originally developed as a single-family residential subdivision. However, the subsequent construction of the interstate and its interchange with U.S. Highway 421 (Michigan Road) has prompted substantial retail and office commercial development. Commercial uses now dominate this area, and industrial uses have been encroaching into the region from the west. Residential uses within this critical area are no longer viable, although some remain.
- **Recommendations:**
 - a. The subdivision should be redeveloped for lower intensity office uses to the west, and higher intensity retail commercial uses to the east. Redevelopment of this area should occur in a coordinated and integrated fashion. The remaining homeowners should be encouraged to sell their properties at fair market prices to allow for redevelopment of the entire area as an integrated commercial center. Individual efforts to convert separate lots into disparate commercial uses should be resisted because piecemeal development will exacerbate the current traffic problems in the area.
 - b. To best accommodate an integrated development containing both office buildings and retail uses, the entire site should be rezoned to C-S. Overall site plans should be filed with the Department of Metropolitan Development. A traffic impact study should be performed and submitted as a part of the rezoning request.

- c. As the area makes the transition from residential use to commercial use, homeowners or other residents remaining in this area should be adequately buffered from any intense commercial uses that are established.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with the Crosby and Brookston soils present in this area. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site has been developed residentially for many years; no significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - Much of this area is not served by sanitary sewers; however, nearby existing development to the south and east utilizes sanitary sewer systems. Because the area's soils are not well suited for septic systems, as the area redevelops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along 92nd street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Michigan Road remain a primary arterial, and that I-465 remain a primary arterial.

Map 13 - Critical Area 4



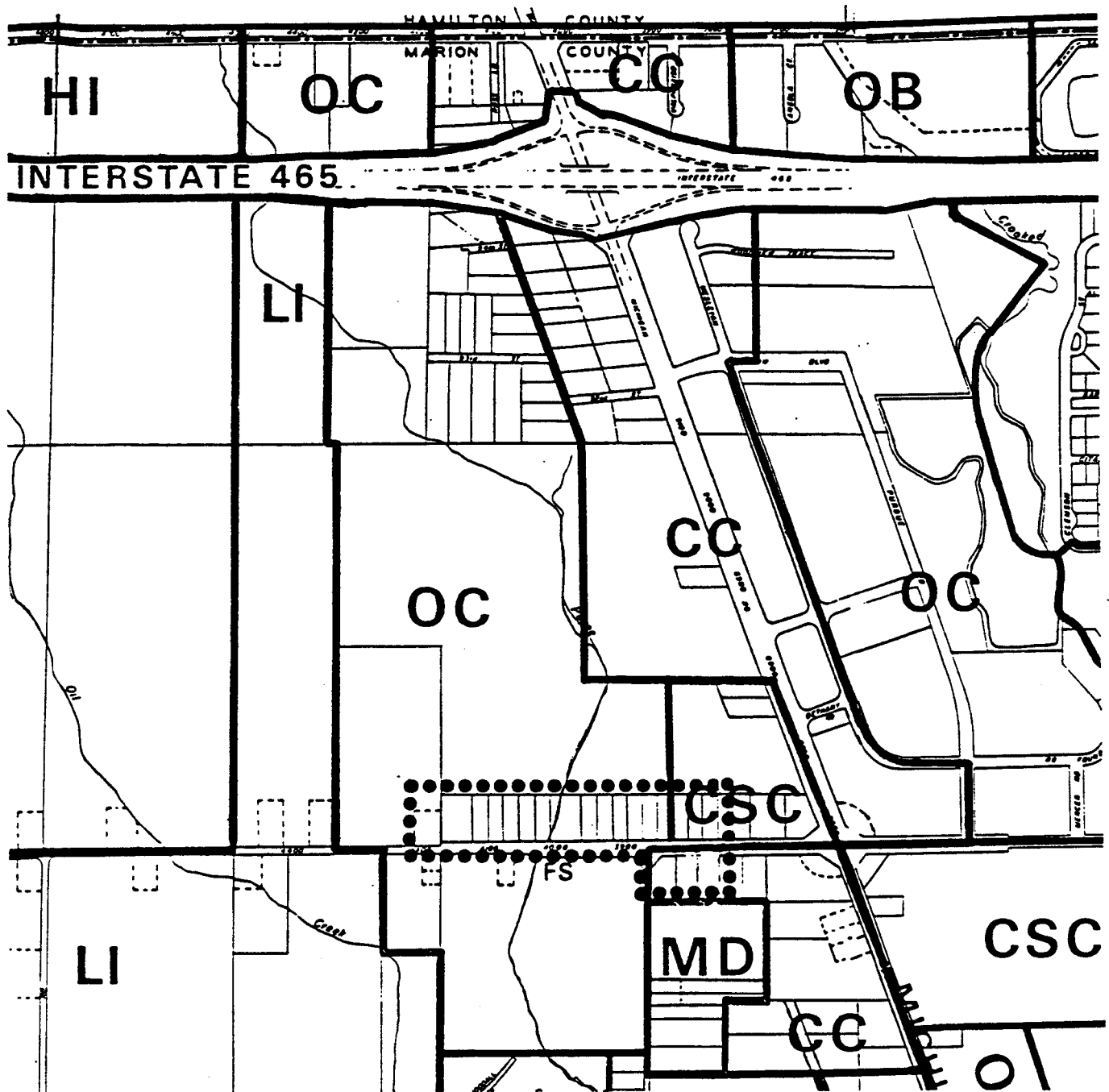
Critical Area 5

- **Location (see Map 14):** Along both sides of West 86th Street, west of Michigan Road to a point approximately 1200 feet west of the Payne Road intersection (approximately 17 acres).
- **Land Use Plan Recommendation:** Office Center [OC]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North: Office Center [OC]
 - East: Community Shopping Center [CSC]
 - South: Office Center [OC]
 - West: Office Center [OC]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** As this segment of West 86th Street has become increasingly commercial in nature, existing single-family homes fronting 86th Street have started to convert to commercial use. However, each property has its own driveway, making access between any two of these properties possible only by way of a heavily travelled arterial roadway.
- **Recommendations:**
 - a. Encourage continued conversion of these properties to office uses. Wherever possible, seek integrated development of office uses on adjoining properties, such that driveway access and signage is consolidated and shared.
 - b. Reserve and protect sufficient area at the front of these properties to accommodate development of a frontage roadway. A frontage road would facilitate access to and between these individual properties and reduce traffic congestion on this stretch of West 86th Street. The engineering for widening 86th Street should provide for the development of a frontage roadway or some other form of shared access to these properties.
- **Additional Data:**
 - a. **Soil Limitations** - The area is composed primarily of Crosby and Miami Silt Loam soils. The wetness and significant shrink-swell properties associated with the Crosby soils, and the low strength of Miami soils could severely

limit the area's development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.

- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site has been developed residentially for many years and is presently under-going a transition to office commercial use; no significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - A sanitary sewer is located along 86th Street, immediately adjacent to this critical area. Because the area's soils are not well suited for septic systems, as the area redevelops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along 86th Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that 86th Street remain a primary arterial and be widened to six lanes. This recommended widening is identified as a Priority A improvement.

Map 14 - Critical Area 5



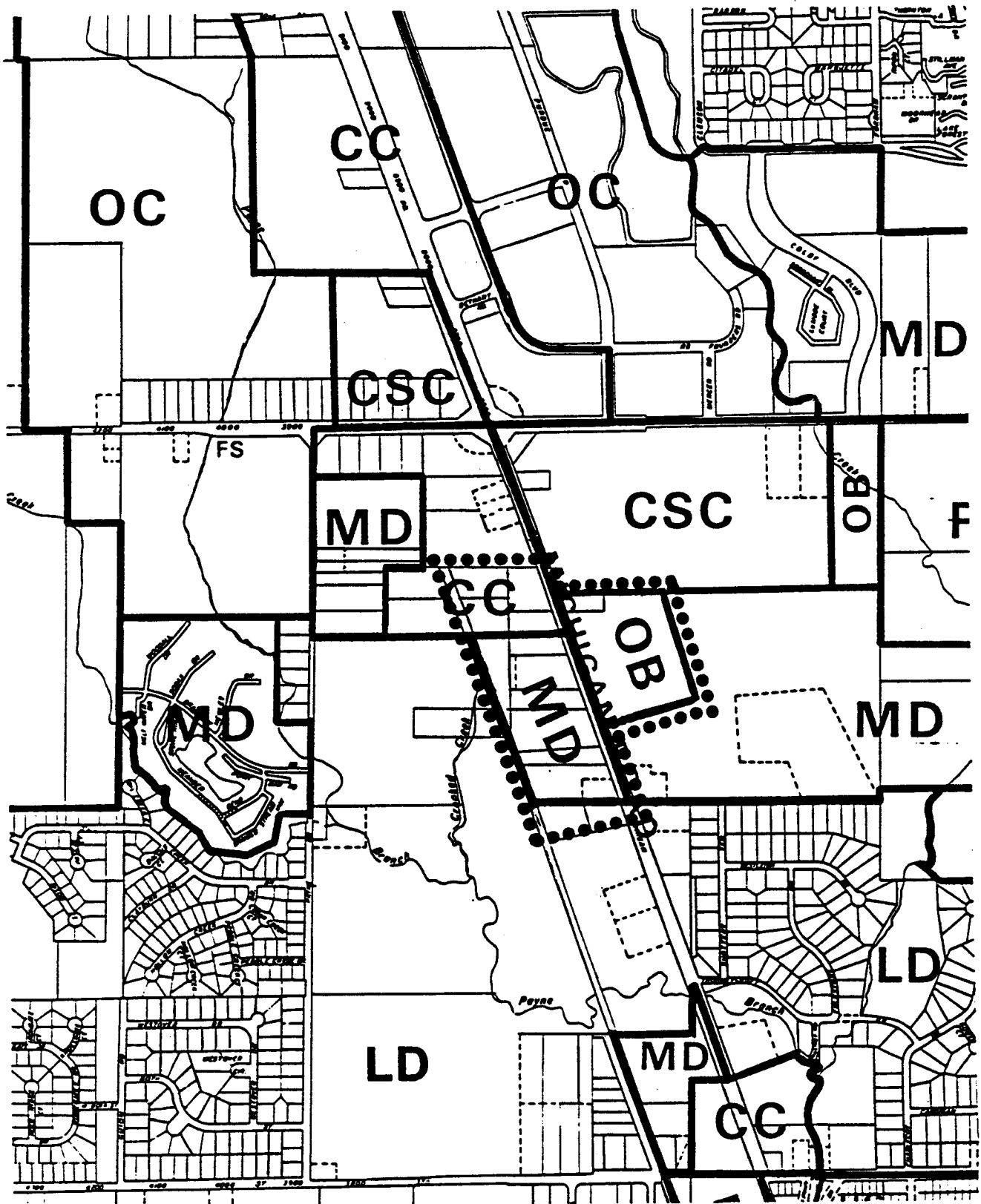
Critical Area 6

- **Location (see Map 15):** Both sides of Michigan Road south of 86th Street to a point approximately 2500 feet south of the intersection (approximately 38 acres).
- **Land Use Plan Recommendations:** Commercial Cluster [CC]; Office Buffer [OB]; and Medium Density Residential [MD].
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Commercial Cluster [CC] and Community Shopping Center [CSC]
 - East:** Community Shopping Center [CSC] and Medium Density Residential [MD]
 - South:** Low Density Residential [LD]
 - West:** Medium Density Residential [MD] and Low Density Residential [LD]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** The intense commercial activity which is clustered around the intersection of West 86th Street and Michigan Road has generated pressure for ancillary commercial development south of it along Michigan Road. Residential uses exist to the East, South, and West which would likely be impacted by the establishment of commercial land uses in this area.
- **Recommendations:**
 - a. This area should develop per the Land Use Plan Map recommendations, with office and medium density residential uses serving as buffers between the retail commercial uses to the north and residential uses to the south.
 - b. Any commercial intrusions into the residential areas south of this critical area should be avoided.
 - c. Establish Office Buffer and Medium Density Residential land uses at the southern perimeter of the area as a permanent southern termination point of the commercial corridor. This will help stabilize nearby residential property values and prevent continued traffic congestion.
 - d. Where Commercial Cluster is the recommended land use (the northwestern corner of this critical area), integration of uses (shared access, parking and signage) is preferred.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site has been partially developed and otherwise impacted for many years; no significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - Sanitary sewer lines exist to both the east and west of this critical area. Because the area's soils are not well suited for septic systems, as the area is developed, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along Michigan Road.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Michigan Road remain a primary arterial and be widened to four lanes. This recommended widening is identified as a Priority B improvement.

Map 15 - Critical Area 6



Critical Area 7

- **Location (see Map 16):** East of Georgetown Road, both north and south of 79th Street, and west of New Augusta Road (approximately 110 acres).
- **Land Use Plan Recommendation:** Low Density Residential [LD]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North: Light Industrial [LI]
 - East: Low Density Residential [LD]
 - South: Low Density Residential [LD]
 - West: Light Industrial [LI]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** The close proximity of potentially incompatible land uses and rapid development of the surrounding area make this a critical area. Individual single-family lots which were platted and developed over 20 years ago front on Georgetown Road and 79th Street, and a grade school exists at the southeast corner of 79th Street and New Augusta Road. To the immediate west is a recently constructed light industrial development and undeveloped industrially zoned land. A recently completed multi-family apartment complex abuts to the south.

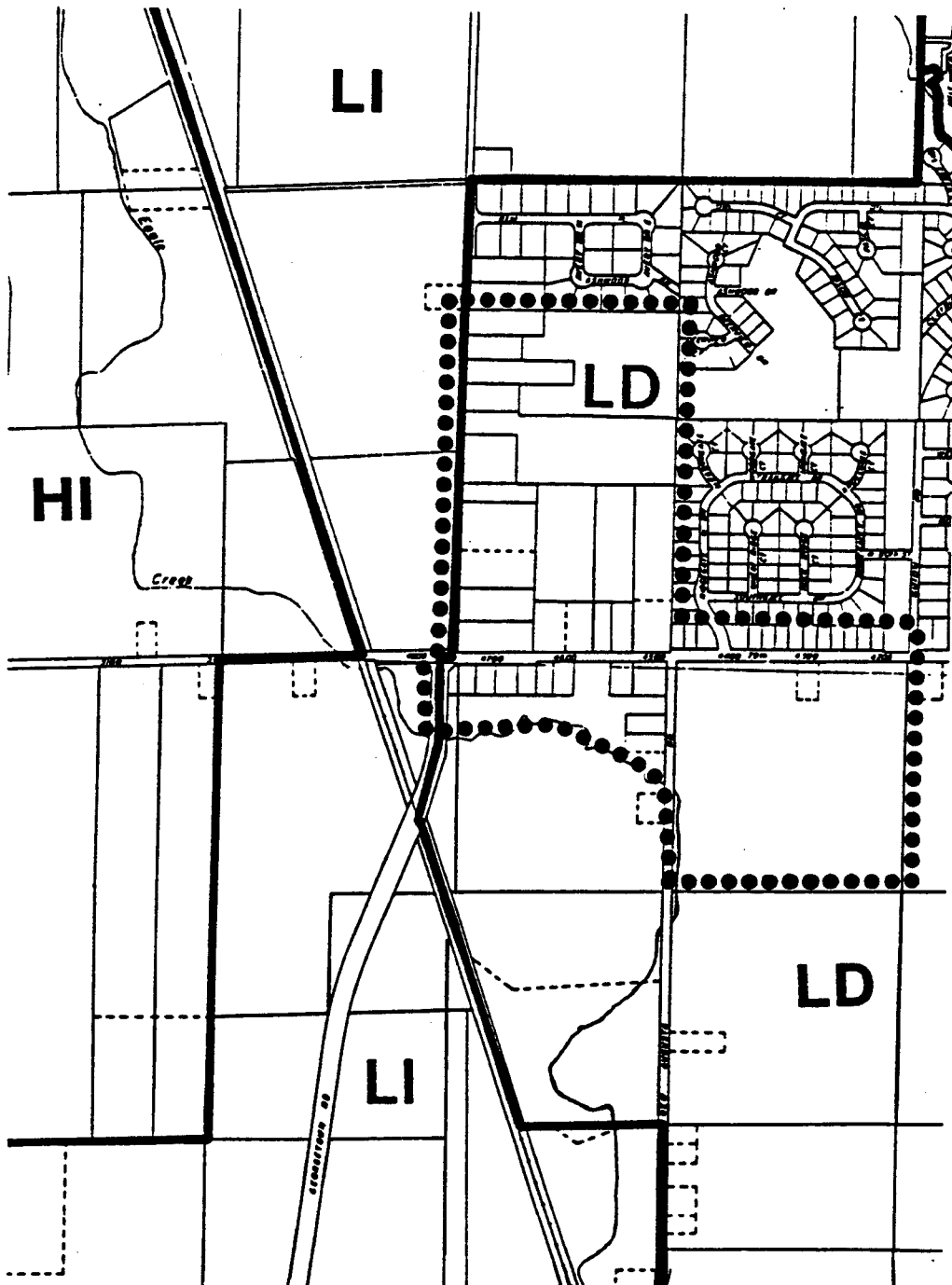
Significant new residential growth is occurring east of this critical area on 79th Street, while additional industrial development is anticipated to the west and southwest. Consequently, as traffic on these roadways increases, pressure may build for some of these individual residential properties to convert to non-residential uses. It is critical that the residential use and character of these properties be maintained due to their proximity to the school and other single-family residential subdivisions in the area.
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
 - b. Encourage industrial developments along the west side of Georgetown Road to set buildings and parking areas back from the right-of-way, and to landscape heavily in their front yards to soften their impact on the residences to the east.

- c. Signage should be modest in scale and lighting should be low intensity for adjacent industrial uses.
- d. Whenever possible, new industrial developments to the immediate west should be designed such that the east facades of buildings are proportionate in scale to the adjacent residences, and they should incorporate other residential design characteristics (eg., building materials, traditional roof lines, adornments, etc.).
- e. Sidewalks should be installed on the east side of Georgetown Road and on both sides of 79th Street to accommodate pedestrians.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site is mostly developed and has otherwise been impacted by man for many years; no significant or unusual environmental limitations appear to be present. Little Eagle Creek skirts the area at its southern border.
- d. **Sanitary Sewers** - Sanitary sewer lines exist in residential developments to both the northeast and south. Because the area's soils are not well suited for septic systems, as the area continues to developed, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along Georgetown Road.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Georgetown Road remain a primary arterial, and that 79th Street remain a secondary arterial.

Map 16 - Critical Area 7



Critical Area 8

- **Location (see Map 17):** Primarily north of West 71st Street, from a point approximately 200 feet west of Georgetown Road to a point roughly 200 feet west of the intersection of 71st and Guion Road, and extending north to include the entire historic community of New Augusta (approximately 116 acres).
- **Land Use Plan Recommendations:** Commercial Cluster [CC]; Light Industrial [LI]; Low Density Residential [LD]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North: Light Industrial [LI]
 - East: Light Industrial [LI]
 - South: Commercial Cluster [CC]; Office Buffer [OB]; Light Industrial [LI]
 - West: Office Center [OC]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** A remnant of the railroad age that prompted its founding in 1855, New Augusta now finds itself in the middle of a variety of competing suburban land uses, including strip retail commercial, light industry, multi-family residential, and single-family housing. New Augusta continues to be viable as a residential community with older homes on rectangular blocks, but its desirability, vitality, and historic integrity are threatened by surrounding non-residential development. It is critical that appropriate land uses are placed in and around this historic town, and that appropriate buffering measures are taken to minimize any adverse impacts.
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map. The vitality and historical integrity of the community hinges on maintaining the existing residences for residential purposes. Conversions to commercial uses should be limited.
 - b. Encourage commercial and industrial developments west and north of New Augusta to set buildings and parking areas back substantially from any adjacent residential properties, and seek commitments to landscape perimeter yards to further soften their impact.

- c. For commercial or industrial uses, especially adjacent to residential uses, only signage and lighting that is moderate in scale and intensity should be allowed. Design guidelines should be established for signage, which would be appropriate for this historic residential community.
- d. To maintain the community's historic character, rehabilitate and reuse existing structures within New Augusta whenever possible, as opposed to demolition and replacing them with new construction. Also, additions to existing structures which are deemed historic or of contributing value to the district should be scaled appropriately, to not overpower the original structures.
- e. Prepare a preservation plan for New Augusta. The Indianapolis Historic Preservation Commission (IHPC) should direct and manage this effort. The IHPC should also design standards and guidelines as part of the preservation plan. After the preservation plan is completed, seek local historic district designation. Local designation would permit IHPC staff to monitor proposed developments within the historic district and review development plans for conformity with the design guidelines (technical support could also be provided by the IHPC staff).
- f. Although properties on the south side of 71st Street are not within the historic district, their development should conform to the character of the area. This will help strengthen the integrity of New Augusta as a historic area, and give 71st Street travellers the sense that they are entering a unique part of Marion County.
- g. Sidewalks should be installed on both sides of Georgetown Road and on both sides of 71st Street in this area to promote and accommodate pedestrian access to area businesses from nearby residential neighborhoods. The north side of 71st Street would likely follow street alignment, widening and intersection improvements.

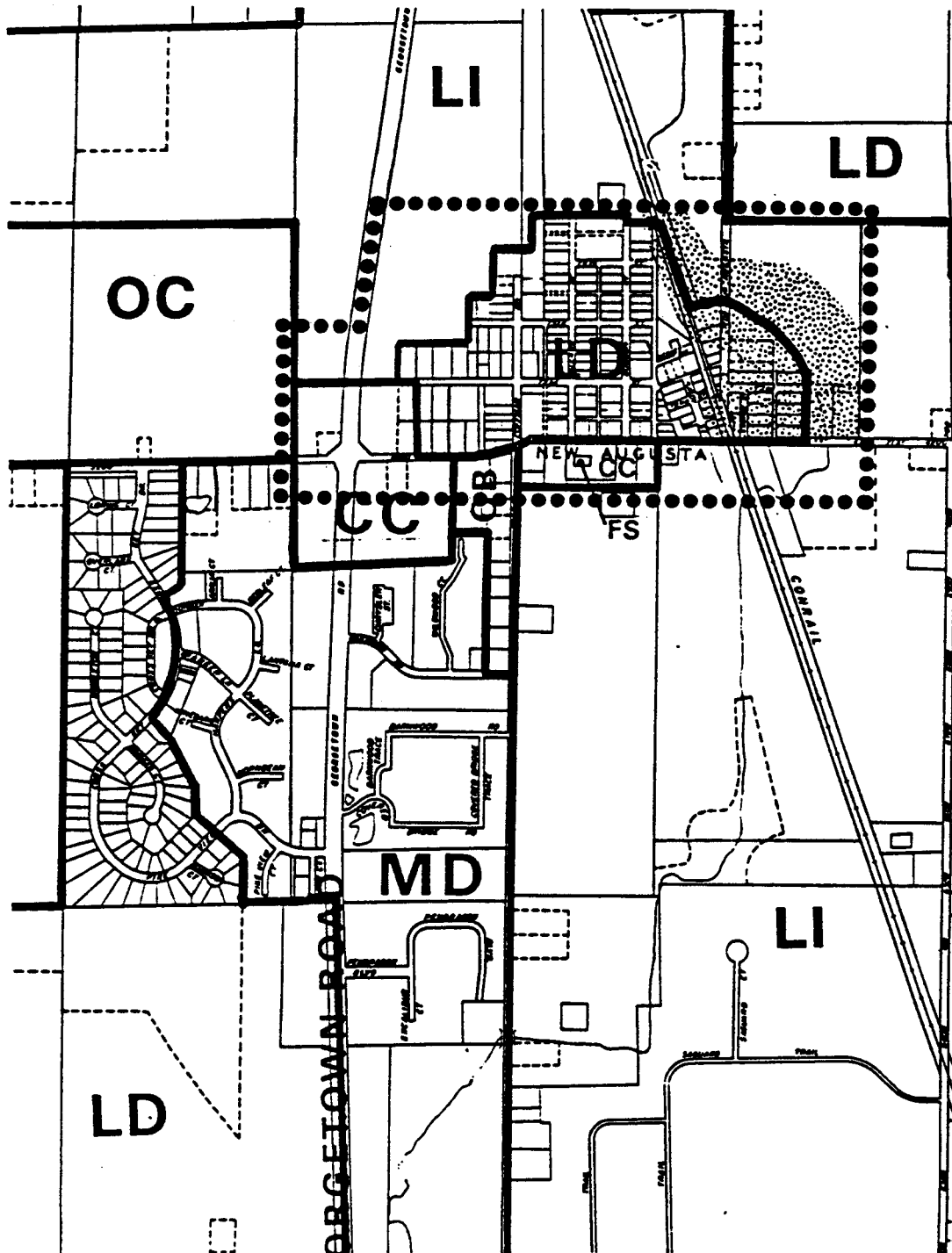
■ Additional Data:

- a. Soil Limitations - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term redevelopment of this area.
- b. Drainage - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development

should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.

- c. **Environmental Constraints** - The area was originally developed in the mid-1800s and has otherwise been impacted by man for over 140 years. Sites adjacent to the railroad may have been used intensively in the past, and should probably be tested for potential soil contamination. No significant or unusual natural features are present within this area which warrant protection.
- d. **Sanitary Sewers** - Sanitary sewer lines now exist within New Augusta. Because the soils are not well suited for septic systems, new development in this area should be sewered as well.
- e. **Water Facilities** - Water service is available along 71st Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Georgetown Road remain a primary arterial, and that 71st Street remain a primary arterial. Additionally, 71st Street is recommended to be widened to four lanes as a Priority C improvement. All additional right-of-way necessary to accommodate this roadway widening should be acquired from the south side of the existing 71st Street right-of-way.

Map 17 - Critical Area 8



Critical Area 9

- **Location (see Map 18):** West of I-465, on both the north and south sides of 71st Street, and East of Shanghai Road (approximately 227 acres).
- **Land Use Plan Recommendations:** Very Low Density Residential [VLD]; Low Density Residential [LD]; Community Shopping Center [CSC]; and Office Center [OC]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Very Low Density Residential [VLD]; Low Density Residential [LD]
 - East:** Interstate
 - South:** Low Density Residential [LD]
 - West:** Very Low Density Residential [VLD] and Low Density Residential [LD]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** Commercial zoning (C-1, C-4, and C-6) exists for a large part of this critical area, yet large lot single-family development and zoning applies to most of the surrounding area. The 71st Street interchange is used heavily by area residents and employees of businesses located in and near Park 100, resulting in a poor level of service for the facility. It is critical that commercial activity on the west side of this interchange be limited to the types and locations recommended on the Land Use Plan map in order to prevent additional traffic congestion and to protect adjoining single-family residential areas.
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
 - b. The recommended Community Shopping Center should be designed and constructed to incorporate residential building features and materials in order to blend well with the area's single-family residential character. The commercial center's signage should be moderate in scale and lighting should be low in intensity.

- c. Allow low density residential uses (and rezone accordingly) within the area currently zoned C-1, southwest of the interchange. The land located at the southwest corner of this interchange was rezoned to C-1 in 1984 to permit the development of an executive office complex. The office complex envisioned at the time of rezoning has since been constructed on another site outside of this critical area. Additionally, since that time a significant amount of land east of this critical area has been planned and zoned for office center uses, thereby reducing the demand for such office use in this part of the critical area. Consequently, the plan recommends that a smaller area be used for an Office Center than what is currently zoned.

Retail commercial uses would be appropriate directly opposite the recommended Community Shopping Center along 71st Street, but they should not extend farther west than Marsh Road. Low Density residential use is more appropriate on the west side and southernmost end of the critical area, adjacent to existing residential subdivisions.

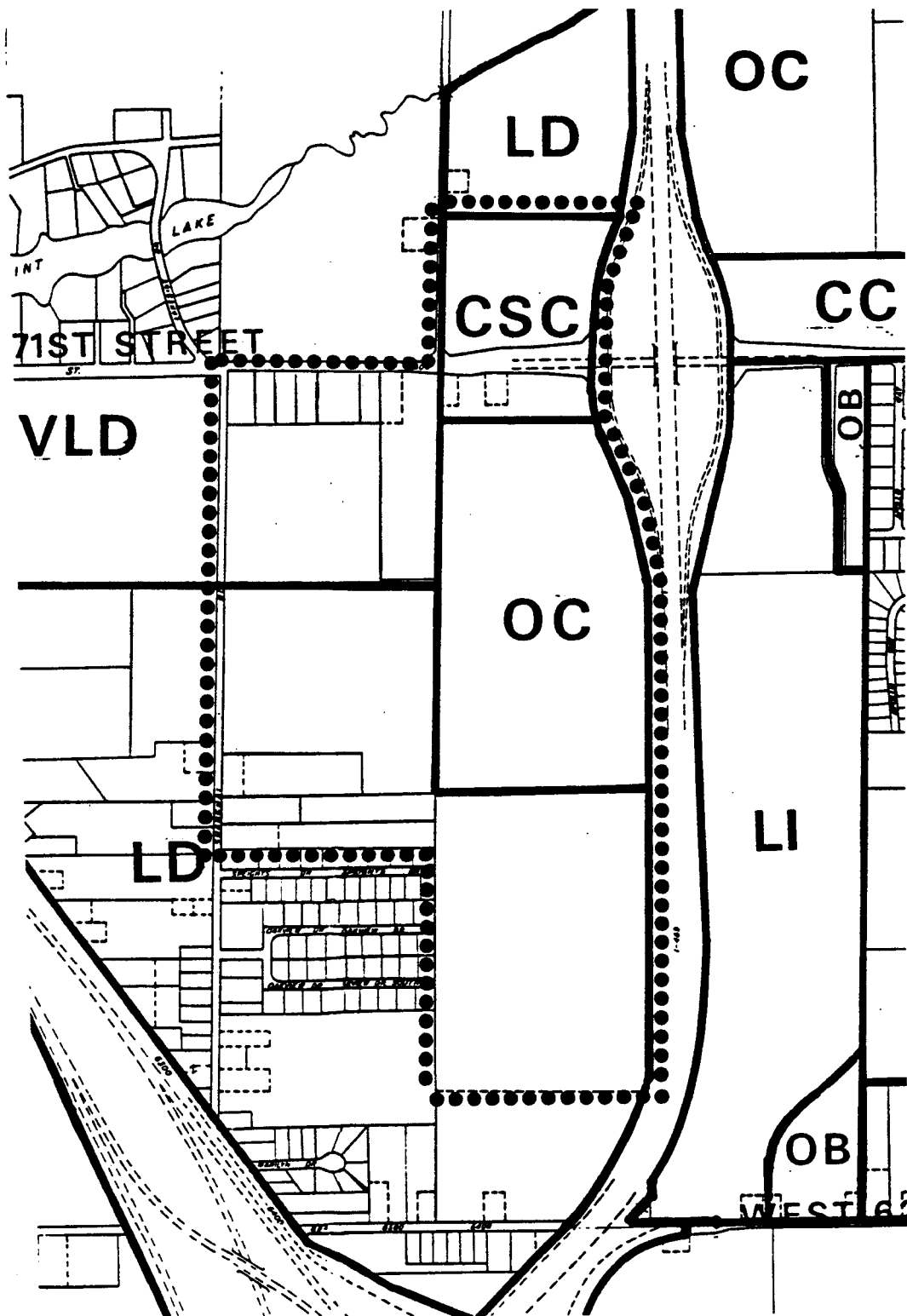
- d. Access to the recommended Office Center should be directly from 71st Street, opposite the planned Community Shopping Center, and opposite Marsh Road.

■ Additional Data:

- a. Soil Limitations - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. Drainage - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. Environmental Constraints - According to the National Wetland Inventory, the area contains a small wetland at its southern end. Although this water feature was man-made (a borrow pit remaining from the construction of the interstate highway), it may play an important environmental role for migratory waterfowl and other species. When this area is developed, care should be taken to protect this wetland area.

- d. **Sanitary Sewers** - A sanitary sewer line exists to the north along 71st Street. Because the area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along 71st Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that 71st Street remain a primary arterial, and that I-465 remain a primary arterial.

Map 18 - Critical Area 9



Critical Area 10

- **Location (see Map 19):** The land areas on either side of a section of West 56th Street, between the I-465 interchange and the Lafayette Road intersection (approximately 146 acres).
- **Land Use Plan Recommendations:** Regional Park - Existing [RP(E)]; Medium Density Residential [MD]; Low Density Residential [LD]; Neighborhood Shopping Center [NSC]; Office Buffer [OB]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Regional Park - Existing [RP(E)]; Office Buffer [OB]; Low Density Residential [LD]
 - East:** Low Density Residential [LD]
 - South:** Medium Density Residential [MD]; Low Density Residential [LD]; Office Buffer [OB]
 - West:** Regional Park - Existing [RP(E)]; Medium Density Residential [MD]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** This area is critical for several reasons. First, 56th Street needs to be widened to accommodate high traffic volumes, and to provide safer conditions for bicyclists traveling to and from Eagle Creek Park. Also, an existing commercial use at the intersection with High School Road threatens the tranquility and integrity of Eagle Creek Park, a nearby grade school, and some existing single-family homes. Similarly, undeveloped commercially zoned land adjacent to the interchange at the west end of the Critical Area was approved originally for commercial uses, and consequently poses a threat to the park. At the eastern end of the critical area, at the Lafayette Road intersection, the integrity of an established residential neighborhood depends on how sensitively commercial uses are developed nearby.

It is critical that 56th Street be widened in a manner that not only accommodates additional traffic, but also treats the adjoining park and residential uses sensitively. It is also critical that office and retail commercial uses be permitted to develop only where they are recommended on the Land Use Plan Map, and that minimal negative impacts occur as a consequence of their development.

■ **Recommendations:**

- a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
- b. Buildings, signs, and other structures should be set back from the right-of-way to allow for an eventual widening of 56th Street. Whenever the roadway is improved, sidewalks should be installed on both sides of 56th Street to accommodate pedestrians and cyclists.
- c. Do not permit the existing convenience mart/gasoline service station at the southwest corner of High School Road and 56th Street to expand or intensify its use beyond its present state by variance or any other means. Additionally, signage and lighting for the commercial use should be moderate in scale and intensity. This use is surrounded by protected zoning districts and sensitive land uses (a park, single-family homes, multi-family housing, and a school). These less intense uses should be thoroughly buffered and protected from the spillover effects of the convenience mart/gasoline service station.
- d. Develop the land adjacent to and southwest of the I-465 and 56th Street interchange with medium density residential use as recommended on the Land Use Plan Map. This site was zoned PK-2 (Park District Two) for highway-oriented commercial uses over 20 years ago, when the 56th Street interchange was envisioned to be expanded to a full interchange, and when the Eagle Creek area's strengths as a residential market and community were still unknown. Subsequently, the planned expansion of the 56th Street interchange has been dropped from the Thoroughfare Plan. Also, residential uses have dominated development throughout the area, including the property immediately south and west of this site which was recently developed for apartments.

Conditions in the vicinity of this site have changed dramatically since the time it was zoned PK-2; thus, any attempts to place commercial uses on this property by way of either rezoning, approval, or variance petitions should be resisted.
- e. Retail commercial uses planned for the intersection of 56th Street and Lafayette Road should be restricted to the northeast corner of the intersection. Less intense uses (not more intense than office buffer), should be placed at the other corners where the available land is significantly smaller in size, and where nearby residents would be more adversely impacted by retail uses.

The southeast corner site in particular should be developed for no use more intense than very low intensity office use. The building should be residential in scale and appearance, with low intensity lighting and minimal signage. Parking should be located in the front yard, leaving the rear yard for appropriate landscaping and screening. Residential use, as permitted by its existing zoning (D-2), is supported by the immediate neighborhood and would also be acceptable.

- f. Residences fronting on Lafayette Road and West 56th Street should be preserved for residential use. Efforts on the part of homeowners to improve or modify these homes to accommodate nonresidential uses (eg., commercial home occupations) should be done such that the primary residential use is not compromised and neighboring homeowners are not adversely impacted. In-home businesses maintained within these homes should not involve or require outdoor use or storage of equipment, supplies, or vehicles.

■ **Additional Data:**

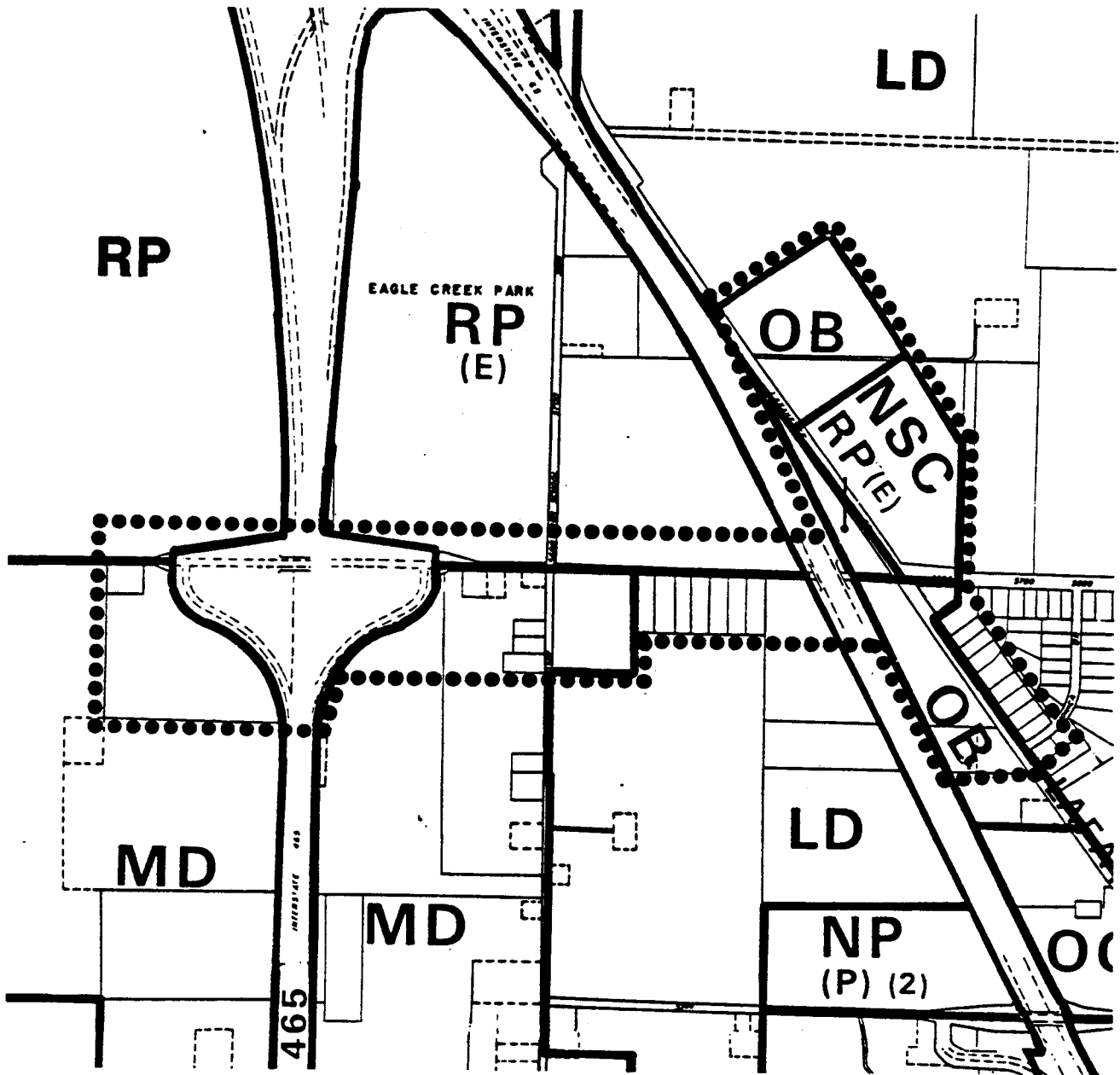
- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting the area's development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - According to the National Wetland Inventory , the area contains a small wetland at its northeastern end. This water feature may play an important environmental role for migratory waterfowl and other species. When this area is developed, care should be taken to protect this wetland area.

Eagle Creek Park lies to the north along most of the area's northern boundary. The park is renowned for its natural beauty and wildlife habitat. As future development is considered for properties adjacent to this unique public facility, care should be given to minimize the impacts of lighting, storm water runoff, noise, and traffic. Lower intensity residential uses are recommended for the properties adjacent to the park because of their lower

intensity and associated levels of impact. The recommendation by the Thoroughfare Plan to widen 56th Street should be reexamined when the Plan is updated in 1993.

- d. Sanitary Sewers - Sanitary sewer lines exist to the west at 56th Street and I-465; and south of 56th Street along High School Road, and along Lafayette Road. Because the area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. Water Facilities - Water service is available along 56th Street.
- f. Transportation - The Official Thoroughfare Plan for Marion County recommends that 56th Street, I-465, and I-65 all remain primary arterials; and that Lafayette Road remain a secondary arterial. A half interchange at I-65 and 56th Street is recommended by the Thoroughfare Plan, although it is not identified as a priority improvement.

Map 19 - Critical Area 10



Critical Area 11

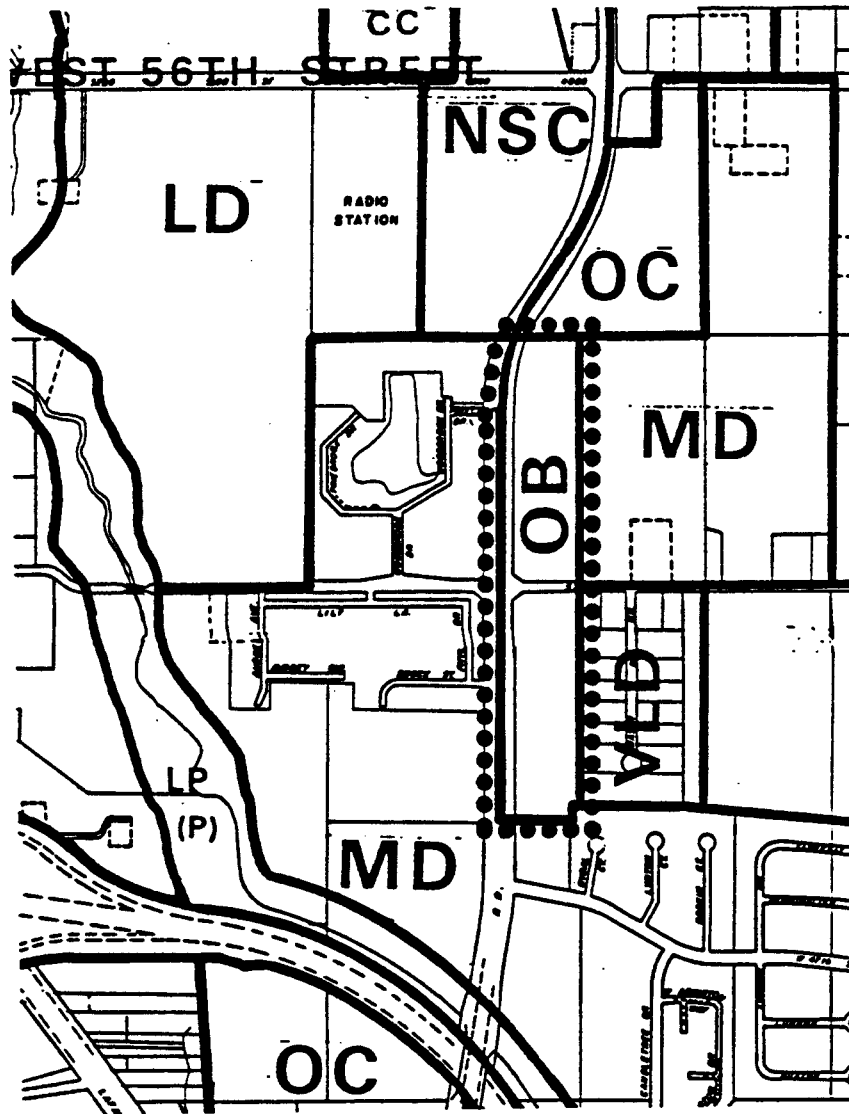
- **Location (see Map 20):** East of Georgetown Road on either side of 52nd Street, between approximately 5400 North and 4900 North (approximately 24 acres).
- **Land Use Plan Recommendation:** Office Buffer [OB]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Office Center [OC]
 - East:** Medium Density Residential [MD]; Very Low Density Residential [VLD]
 - South:** Medium Density Residential [MD]
 - West:** Medium Density Residential [MD]
- **Stage of Development:** 6 (Developing Suburban Area)
- **Why Critical:** This area is located in the middle of an area planned, zoned, and partially developed for medium density residential use. However, it is also adjacent to a very low density residential subdivision on the east, and a small office center on the north. The extensive frontage along Georgetown Road combined with limited lot depth and close proximity to the 56th Street commercial node suggest that office uses would be appropriate for this area. It is critical that the office uses develop in a manner which is complementary to the single-family residences to the east. Additionally, access to Georgetown Road should be restricted to minimize the disruption of traffic flow along that arterial roadway.
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
 - b. Encourage office developments to set buildings and parking areas back from the right-of-way, and to obtain access from shared frontage access drives off of 52nd Street. The drive for the north parcel should intersect 52nd Street opposite the access drive for the south parcel.
 - c. Heavy landscaping should be utilized along the east perimeter yards to soften the impact of the office buildings and parking areas on the single-family residences to the east.
 - d. Signage should be modest in scale and lighting should be of low intensity.

- e. The office structures should be appropriately designed such that overall building mass and the facades are proportionate in scale to the adjacent residences. Also, residential design elements such as gabled or hipped roof lines, liberal use of windows, and masonry construction should be incorporated to further complement the surrounding area's residential character.
- f. Zoning should be consistent with the plan recommendations. The north parcel is currently zoned C-1, which conforms with the Land Use Plan Map recommendation. The south parcel is currently zoned D-A (Dwelling-Agriculture), and should be rezoned to C-1.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site is surrounded by residential development, and has previously been used for agriculture. No significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - A sanitary sewer line exists to the west at 52nd and Georgetown Road. Because the critical area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along 52nd Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Georgetown Road remain a primary arterial.

Map 20 - Critical Area 11



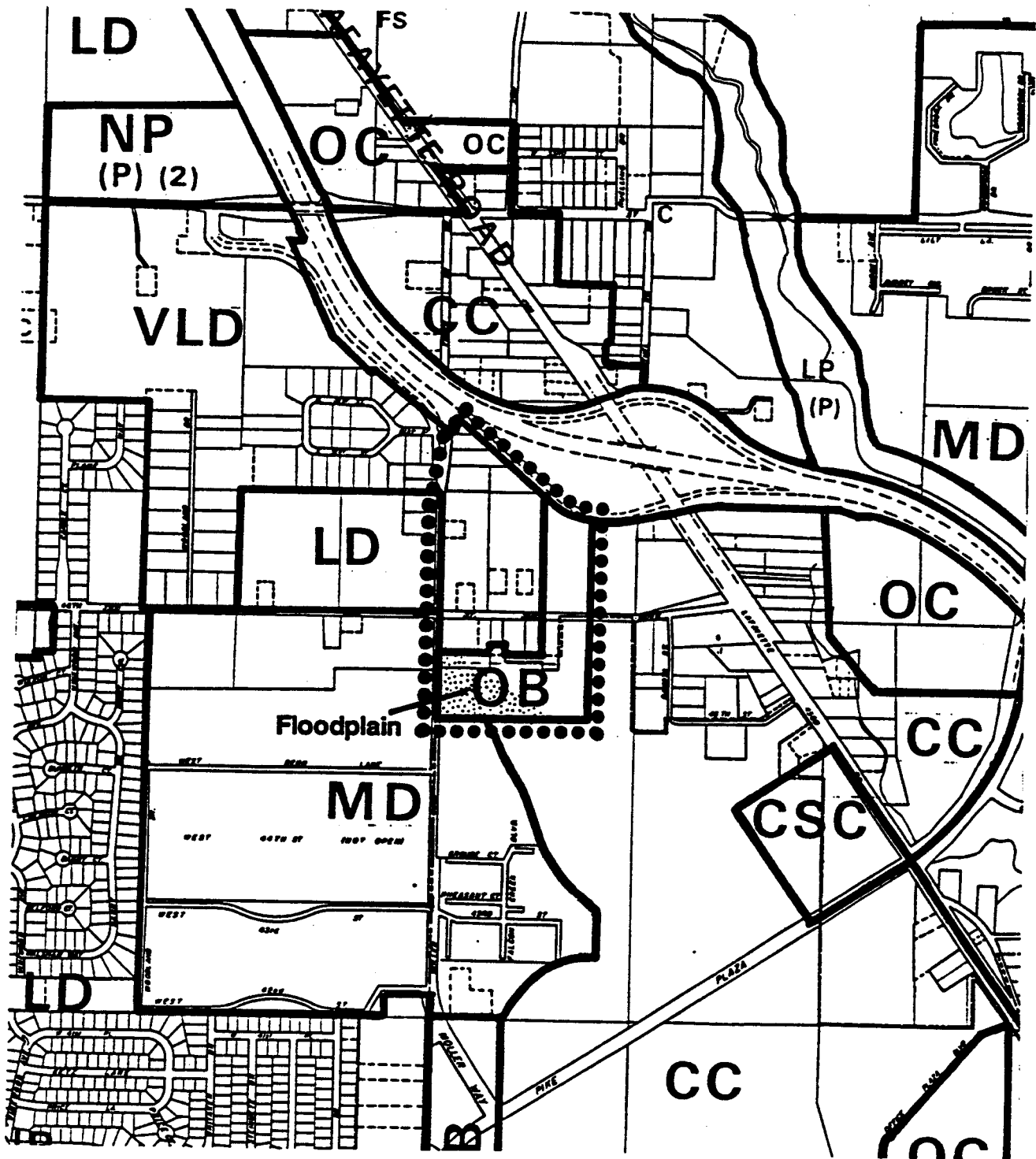
Critical Area 12

- **Location (see Map 21):** West of Lafayette Road and east of Moller Road, on either side of 46th Street (approximately 35 acres).
- **Land Use Plan Recommendations:** Very Low Density Residential [VLD]; and Office Buffer [OB]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Interstate Highway
 - East:** Commercial Cluster [CC]
 - South:** Medium Density Residential [MD]; Commercial Cluster [CC]
 - West:** Low Density Residential [LD]
- **Stage of Development:** 5 (Established Suburban), and 6 (Developing Suburban Area)
- **Why Critical:** Existing single-family development in this area is within very close proximity of the I-65 interchange with Lafayette Road and an intense commercial corridor. The long-term viability of these residential properties is dependent on the establishment of an effective buffer use between them and the intense commercial uses.
- **Recommendations:**
 - a. The southern and eastern portions of this critical area should be developed as an office buffer area in order to protect the residences to the west from the intense auto-related commercial activities to the east.
 - b. Signage and lighting for the office buffer uses should be appropriately modest in scale and low in intensity adjacent to the residential uses.
 - c. Office buildings should be designed to be proportionate in scale to the adjacent residences. Also, the buildings should incorporate other residential characteristics (e.g., building materials, traditional roof lines, and adornments).
- **Additional Data:**
 - a. **Soil Limitations** - The area is composed primarily of Crosby, Miami, and Shoals Silt Loam soils. The Crosby soils are characterized by wetness and

significant shrink-swell properties, and the Shoals soils are susceptible to flooding. The area's development potential with regard to basements, foundations, drives, and septic systems is consequently very limited by its soil conditions. Sanitary sewers as well as appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.

- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Additionally, Falcon Creek, which traverses this area, will flood occasionally. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site is bisected by Falcon Creek. Proper stormwater detention facilities should be incorporated with new developments to ensure that stormwater runoff does not substantially increase the stream's nutrient or sediment load. No other significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - No sanitary sewer lines exist within this critical area, although a line is located to the east at Lafayette Road. Because the critical area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available east of the area along 46th Street.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that 46th Street and Moller Road remain secondary arterials.

Map 21 - Critical Area 12



Critical Area 13

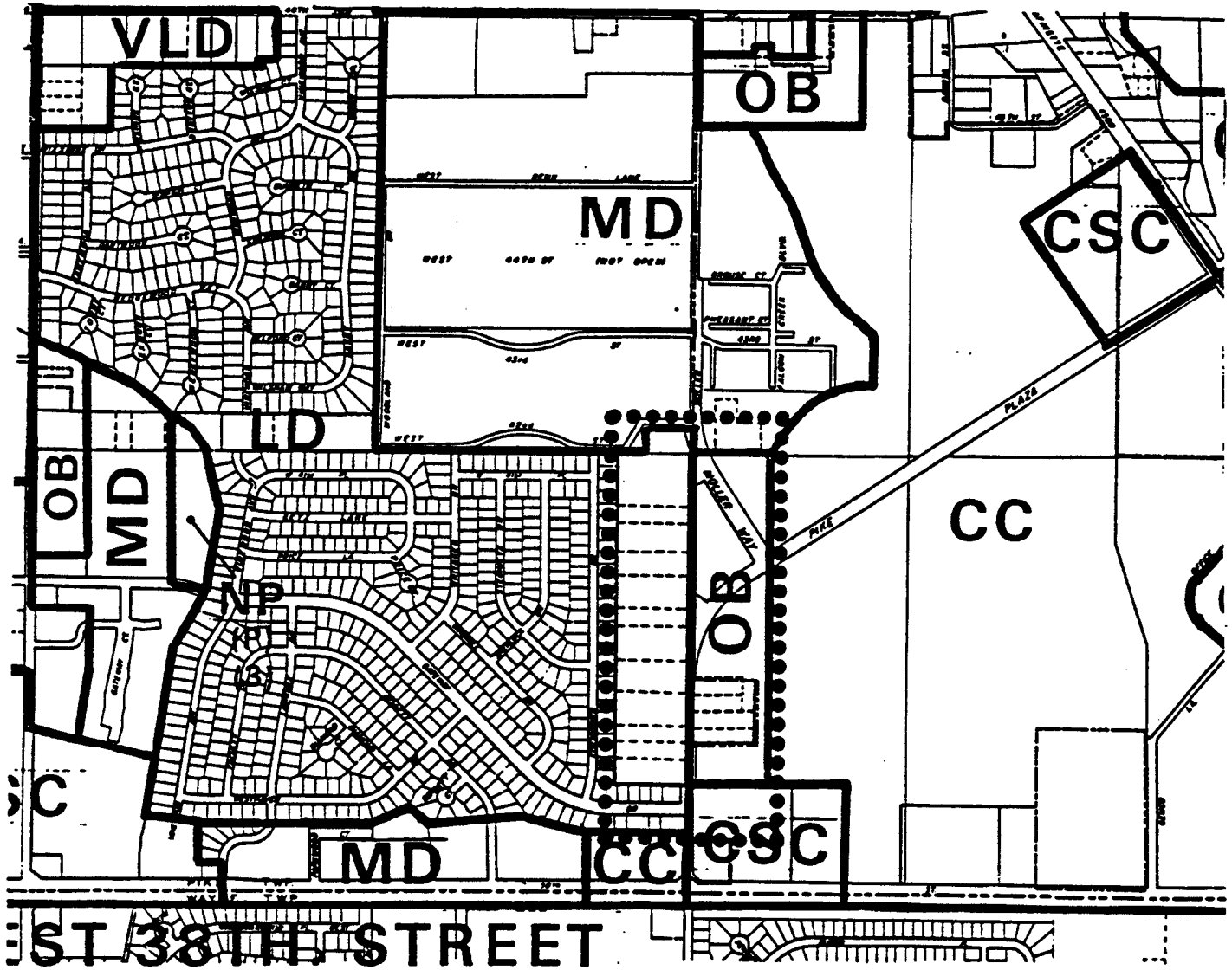
- **Location (see Map 22):** Both sides of Moller Road, from 38th Street on the south to approximately 4200 North Moller Road on the north (approximately 50 acres).
- **Land Use Plan Recommendations:** Low Density Residential [LD]; Office Buffer [OB]; Commercial Cluster [CC]; and Community Shopping Center [CSC]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North: Medium Density Residential [MD]
 - East: Commercial Cluster [CC]
 - South: Commercial Cluster [CC]
 - West: Low Density Residential [LD]
- **Stage of Development:** 5 (Established Suburban Area)
- **Why Critical:** Situated immediately west of the Lafayette Square Shopping Center commercial district, this area presents the final opportunity to appropriately buffer the single-family residential uses to the west from the impacts of retail commercial activities to the south and east. Community Shopping Centers (existing and planned), a commercial fitness center, and auto related uses dominate the area immediately east of this critical area; multi-family housing predominates to the north. Nevertheless, from Gateway Drive north to the intersection with Moller Way, this stretch of Moller Road is clearly residential in character. There are twenty-two individual single-family residential properties with lot frontage on this stretch of Moller Road. Eighteen of these would be greatly impacted by the establishment of anything more intense than office buffer uses on the east side of Moller Road. The remaining four are located on the east side, and could be converted to office uses.
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
 - b. Encourage Office Buffer uses on the east side of Moller Road to set buildings and parking areas back substantially from the right-of-way, and to landscape heavily along Moller Road to soften their impact on the residences to the west.

- c. Signage and lighting for the office buffer uses should be moderate in scale and intensity. Only non-illuminated ground signs would be appropriate opposite the single-family homes.
- d. The triangular parcel bounded by Moller Road, Moller Way, and Pike Plaza Road should be developed so that access is from Moller Way. No access should be obtained from Moller Road on its west side.
- e. Facades of office buildings should be proportionate in scale to the adjacent residences. The west-facing facades should incorporate other residential characteristics as well (building materials, traditional roof lines, and adornments) to maintain the area's residential character.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - The site was originally developed residentially over 30 years ago, and was previously been used for agriculture. No significant or unusual environmental limitations appear to be present.
- d. **Sanitary Sewers** - A sanitary sewer line exists to the south at 38th Street, and another is located at the northwest corner of the area along Pike Plaza Road. Because the critical area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along Moller Road.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that Moller Road and Pike Plaza Road remain secondary arterials.

Map 22 - Critical Area 13



Critical Area 14

- **Location (see Map 23):** The extreme southeast corner of the Township, from Guion Road to the Washington Township line, and from approximately 4000 north to 38th Street (approximately 44 acres).
- **Land Use Plan Recommendations:** Low Density Residential [LD]; and Very Low Density Residential [VLD]
- **Surrounding Land Uses Recommended in the Comprehensive Plan:**
 - North:** Low Density Residential [LD]; Very Low Density Residential [VLD]
 - East:** Very Low Density Residential [VLD]
 - South:** Interstate Highway
 - West:** Light Industrial [LI]
- **Stage of Development:** 5 (Established Suburban Area)
- **Why Critical:** This site represents an infill development opportunity. Within one mile in any direction, there are no other infill sites of this acreage, visibility, or proximity to highways and shopping areas.

The site abuts existing single-family subdivisions to the north, east, and west, and is recommended for Low and Very Low Density residential development. The high visibility and frontage on the 38th Street/I-65 highway corridor could prompt landowner(s) or developers to speculate on establishing higher intensity uses on the site which could negatively impact the adjoining established residential areas.

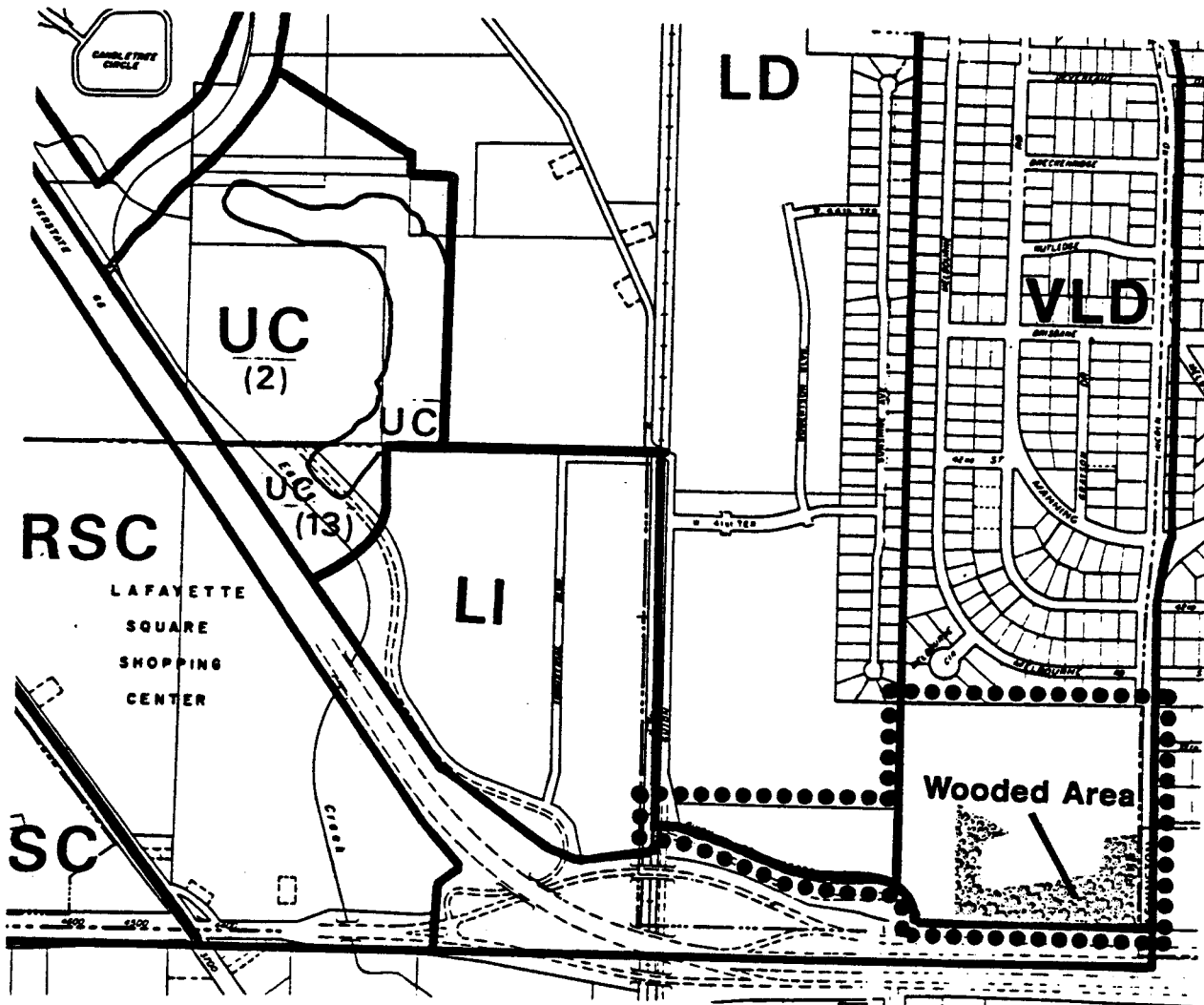
- **Recommendations:**
 - a. Adhere to the land use recommendations made in the Pike Township Comprehensive Land Use Plan Map.
 - b. Encourage cluster subdivisions to preserve a natural woodland buffer along the southern and eastern edges of the site, and to concentrate the greater number of units toward the west.
 - c. Access should be from 38th Street (Westbound only) and Guion Road via a connector roadway; or, if a connection can be made to either of two stubbed streets to the east (39th Street and 39th Place), access from Kessler Boulevard North Drive would be possible. If an access roadway were to be

constructed connecting Kessler on the east with Guion on the west, it could promote use of local roadways by commuters and others as a short-cut to and from Interstate I-65. This issue should be addressed by using discontinuous roadways, or by limiting access to one side of the site or the other.

■ **Additional Data:**

- a. **Soil Limitations** - The area is composed primarily of Crosby and Brookston soils. These soils are characterized by wetness and significant shrink-swell properties, thereby severely limiting their development potential with regard to basements, foundations, drives, and septic systems. Sanitary sewers and appropriate site preparation and construction measures should be used to offset these limiting conditions and help ensure the successful long-term development of this area.
- b. **Drainage** - Drainage problems are commonly associated with Crosby soils due to low permeability and a high water table. Potential development should be accompanied by a thorough analysis of on- and off-site drainage impacts, as well as drainage plans that follow the requirements of the City's Drainage and Sediment Control Ordinance.
- c. **Environmental Constraints** - A moderate woodland area is located within this area at its southeastern side. when the area is rezoned or otherwise planned for development, a tree preservation plan should be devised.
- d. **Sanitary Sewers** - Sanitary sewers exist to the northwest, within an existing subdivision. Because the area's soils are not well suited for septic systems, as it develops, sanitary sewers should be provided.
- e. **Water Facilities** - Water service is available along the western perimeter of the area.
- f. **Transportation** - The Official Thoroughfare Plan for Marion County recommends that 38th Street and I-65 remain primary arterials, and that Guion Road remain a secondary arterial. 38th Street is recommended to be widened to six lanes as a Priority A improvement.

Map 23 - Critical Area 14



Conclusion

The Pike Township Comprehensive Land Use Plan has been developed through a systematic and highly participatory planning process. The Division of Planning staff initiated the planning process, gathered and analyzed township data, and led a series of discussion-oriented evening meetings with the Pike Township Planning Committee. The plan's recommendations reflect thorough examination of township population and land use characteristics as well as open discussion of planning and development goals. The recommendations of the plan map and plan narrative will help guide the future development of Pike Township.

The plan will be evaluated in the future to determine when there is a need for further revision. Some of the factors that may indicate the need for a revision include fundamental changes in land use or population, rapid development of all or part of the township, and extension of major sewer or water lines. Periodic revisions can be made via the adoption of new critical areas or subarea plans, thereby keeping the Pike Township Plan current without development of an entirely new township plan. When the time comes to revise the township plan as a whole, the process will again be thorough, highly participatory, and reflective of the community's planning and development goals.

Appendix A

Pike Township Planning Committee

The Planning Committee was an open membership committee. The Division of Planning initiated formation of the Planning Committee by inviting neighborhood organization presidents, school board members, City-County Council members, Metropolitan Development Commission members, and other township leaders. The Division advertised the planning process and meetings in local media and also relied on committee members to communicate with others.

Members of the Planning Committee participated actively throughout the planning process. Thus, plan recommendations reflect extensive committee discussion and analysis. The Division of Planning staff thanks the Planning Committee members for their invaluable participation and patience throughout the months of review and revision that resulted in the Pike Township Comprehensive Land Use Plan Map and this Pike Township Comprehensive Land Use Plan narrative. The following is a list of Pike Township Planning Committee members who attended committee meetings.

Kathy Arauco
Charles S. Berry
Don Beyson
Gerry Bissell
K. Blackburn
Kathleen Bodell
Molly Boyce
Lucille Crooks
Nelson Crooks
Debbie Estes
Mike Gadus
Stan Geiser
Gordon Gilmer
John Hart
Howard Harvey
Sharon Hause
Dick Hawkins
Virginia Hawkins
Rob Hedding
Charles Jordan
Sid Lewis
Paul Ludwig

Golam Mannan
Dorothy Miller
David Moloy
John E. Moody
Christine Patty
Jeff Paul
Tom Ransburg
Kevin Russell
Robert Scannell
Marilyn Smith
John Teter
Bill Thomas
Jessie E. Thomas
Bob Trivers
Dennis Troth
Elaine Troth
Mike Tyler
Michael Wukmer
Bill York
Dean Ziegler
Greg Zubek



Elected Officials

Stephen Goldsmith, *Mayor*

City-County Councillors and Districts

Gordon Gilmer, 1
William Schneider, 3
Linda Beadling, 5
Stuart Rhodes, 7
Monroe Gray, 9
Rozelle Boyd, 11
Cory O'Dell, 13
Mary B. Moriarty, 15
Jeff Golc, 17
Kenneth Giffin, 19
Frank T. Short, 21
David Smith, 23
Dr. Philip Borst, 25
Ron Franklin, AL
Stephen R. West, AL

Dr. Beurt SerVaas, 2
William Dowden, 4
Elwood E. Black, 6
Randy Shambaugh, 8
Paul Jones, 10
Betty Ruhmkorff, 12
Z. Mae Jimison, 14
Maggie Brents, 16
Phillip Hinkle, 18
Timothy M. Mullin, 20
Susan Williams, 22
Beulah Coughenour, 24
Carlton E. Curry, AL
W. Tobin McClamroch, AL

Metropolitan Development Commission

William R. Brown
Jack Hall, M.D.
Mary Ann Mills
Michael Rodman
Randolph L. Snyder

James J. Curtis, Sr.
Dorothy Miller
Walt Niemczura
Julie P. Scott

Project Coordination

Nancy Silvers, *Deputy Mayor and Director for Dept. of Metropolitan Development*

Elizabeth Williams, *Pike Township Administrator*

Department of Metropolitan Development, Planning Division

Jon Meeks, *Administrator*
Thomas Bartlett, *Senior Planner*
Gina Bush, *Graphic Designer*
Steven Cunningham, *Senior Planner*
William Gentry, *Senior Planner*
Jay Getz, *Planner*
Keith Holdsworth, *Senior Plannner*

Mike Peoni, *Senior Planner*
Stuart Reller, *Master Planner*
Phil Pettit, *Drafting Superintendent*
Darrell Walton, *Draftsman*
Anna Davenport, *Secretary*
Carole Wilburn, *Secretary*

Participating Agencies

City of Indianapolis Departments:
Metropolitan Development
Parks and Recreation
Public Works
Transportation
Indiana Dept. of Natural Resources

Indiana Dept. of Transportation
Indianapolis International Airport
Indianapolis Water Company
Marion County Health Department
Metropolitan School District of Pike Township
Pike Township Fire Department